

# APODACA BLUEPRINT

A Community Blueprint Policy Plan  
July 2, 2018



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**Adopted by the Las Cruces City Council  
on July 2, 2018**



## ACKNOWLEDGMENTS

The Apodaca Blueprint has been developed by the City of Las Cruces with the technical assistance of Halff Associates, Inc. The following individuals are specifically recognized for their significant contribution to the preparation of this guiding document.

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# **I. APODACA BLUEPRINT PARAMETERS**

# Community Blueprint Planning Initiative

The Apodaca Blueprint is a neighborhood planning effort initiated by the City of Las Cruces to promote the vitality and investment in the neighborhoods to the east and southeast of Apodaca Park. As existing neighborhoods age, they experience challenges relating to infrastructure condition, public services, character and aesthetics, and housing condition and choice. The Apodaca Blueprint will recommend “neighborhood conservation” strategies to maintain or improve the condition of property, and quality of life within Apodaca area neighborhoods.

A significant consideration when undertaking of the Apodaca Blueprint planning effort was the uncertainty surrounding development plans for the former Las Cruces Country Club, a property that is amid a major transformation. While the redevelopment of this property can provide an economic boost for the community, major changes to land uses and development intensities can also have unintended consequences for those residing near the site.

## COMPREHENSIVE PLANNING THEMES

The City of Las Cruces Comprehensive Plan 2040 along with Plan4LasCruces (an unadopted update to the comprehensive plan) identify four themes that frame the vision for the City<sup>1</sup>. Those themes are: Healthy Communities, Community Character, Sustainable Growth, and Economic Prosperity. These four themes encompass the variety of functions that cities are tasked to administer by the local community. The Apodaca Blueprint considered these planning themes by using them as guiding principles throughout Blueprint development.

**PLAN4LASCRCUCES: HEALTHY COMMUNITIES** relates to the following three principles:

- ‘Provide high quality parks, recreation, open space amenities and community services to promote healthy lifestyles and overall community wellbeing,’
- Provide a range of housing alternatives within neighborhoods that are safe, well built, clean, comfortable and affordable at varying economic levels to promote equity and choice,’ and
- ‘Provide for a safe, efficient, effective, convenient and connected multi-modal transportation system which promotes healthy living and provides mobility alternatives.’

### SUB THEMES:

- Balanced development
- Great parks & recreation
- Wide-ranging community facilities & services
- Multiple mobility options and connections
- Healthy & safe environment
- Healthy food



**PLAN4LASCRCUCES: ECONOMIC PROSPERITY** relates to the following principle:

- ‘Establish a diversified sustainable economy.’

### SUB THEMES:

- Economic diversity
- Business & industry support
- Ready workforce & environment



**PLAN4LASCRCUCES: COMMUNITY CHARACTER** relates to the following principle:

- ‘Promote a healthy community through design of the built environment using landscaping, architecture, open space and conservation.’

### SUB THEMES:

- Enriched heritage
- Flexible design & positive image
- Open space connectivity
- Aesthetics & maintenance



**PLAN4LASCRCUCES: SUSTAINABLE GROWTH** relates to the following principle

- ‘Guide the location of future growth and development to best balance the positive aspects of the social, economic and environmental aspects that make the city a great place.’

### SUB THEMES:

- Vibrant planning areas, neighborhoods & districts
- Managed growth
- Well suited utilities, infrastructure & resources



<sup>1</sup> The information contained in a fifth theme (Operational Support), included in the City of Las Cruces Comprehensive Plan 2040 is proposed to be integrated into the four Plan4LasCruces themes.

# Apodaca Blueprint Vision and Objectives

The vision and objectives of a plan set the aspirational direction for the what the planning area will one day become. These plan elements are developed from synthesizing public input, stakeholder guidance, and City direction. This is essentially the “blueprint” portion of the Apodaca Blueprint. Establishing the vision and objectives of a plan guides the formation of recommendations. The subsequent recommendations will be translated into policies and actions that can be used to measure both the success and progress of plan implementation.

The following vision statement and goals were developed in consideration of the multitude of participants, including area residents and business, City staff, public officials, and others.

## VISION STATEMENT

***Strengthen the long-term health of Apodaca Area neighborhoods by balancing the conservation of existing residential character with investment opportunities that diversify overall housing choice and resident’s access to local amenities and services.***

## GOALS

- Define future land use and development characteristics of property in the planning area.
- Facilitate new development that jointly benefits property owners, investors, surrounding neighborhoods, and the city as a whole.
- Mitigate the impacts of development on surrounding residential areas.

- Identify potential enhancements to area-wide public infrastructure and services.
- Address miscellaneous issues identified during the planning process to ensure the long-term vitality of planning area neighborhoods.

## THE PLAN

The Apodaca Blueprint includes future development plans and maps, and supporting conceptual site plans, for the overall Apodaca Planning Area and two “focus areas.” These future development plans have been prepared to provide surrounding residents with greater certainty in how new development can be integrated into their community in a complimentary manner. These plans were developed with input from neighborhood and community stakeholders.

The overall scope of the Blueprint planning process includes six distinct components:

- Project initiation and site evaluation
- Public engagement
- Existing conditions and assessment
- Master plan design workshop
- Master plan refinement
- Plan adoption

These six components were included to ensure that the planning process was conducted in an informed and transparent manner, and included built-in mechanisms where public input/comment would be gathered during key points in the planning process.

## ADDITIONAL TASKS

The Apodaca Blueprint is a policy document. It is an extension of the comprehensive planning process - particularly Goal 42 of Plan4LasCruces - which advocates the preparation of neighborhood-specific plans to guide future growth and development.

As part of the formal development process, for any development site within the planning area however, further studies, reports, assessment, and construction documentation will be necessary. This includes traffic impact analysis, drainage reports, subdivision plats, etc.



IMAGE 1: The Three Crosses Memorial is one of many significant Planning Area site features that can be enhanced in conjunction with the future development process.



# Apodaca Planning Area

The Apodaca Blueprint Planning Area is roughly 730 acres in size. It is generally bounded by Interstate 25 on the east, Mulberry Ave. to the south, N. Mesquite Street to the west, and U.S. 70 (N. Main St.) and the Alameda Arroyo to the north. Two additional focus areas of approximately 158 acres and 77 acres are located on the western and eastern boundaries of the planning area, respectively.

- **Focus Area A.** The Las Cruces Country Club property, which closed its operation in 2011, has remained vacant since its shutdown. There has been development activity to construct a medical facility on a portion of the property, however much of the remainder of the property has been marred in uncertainty. This focus area includes Apodaca Park.
- **Focus Area B** The Villa Mora property is an underutilized and obsolete detention basin that may be best characterized as open space. A large berm on the western portion of the property and the area’s existing vegetation are the prominent site attributes. The construction of the Las Cruces Dam in 1975, by the U.S. Army Corps of Engineers, significantly reduced the overall demand for storm water detention capacity at this site. While the property still contributes to storm water storage, the site is under-programmed.



**APODACA PARK.** This 27 acre park space is a popular destination for area residents and the general Las Cruces public. It is located in the west portion of the Planning Area and is accessed from both E. Madrid Ave. and N. Solano Drive. The park includes two baseball fields, tennis courts, and playgrounds. There are a number of large shade trees found throughout the park. It is also home to the Dineh statue.



**MADRID AVENUE.** E. Madrid Avenue traverses the Planning Area as the primary east-west route, connecting both the Las Cruces Country Club/Apodaca Park and Villa Mora focus areas (Focus Areas A & B, respectively). A variety of uses may be found along this street, including restaurants, businesses, apartments, single-family homes, and the Loma Heights Elementary School.



**AREA TRAILS.** The Outfall Channel Trail and the Triviz Trail bound the plan area to the north and the east. These trails provide strong access for active transportation opportunities to neighboring areas of Las Cruces. As these trails are located along the periphery of the Planning Area, access to these resources requires connections into Apodaca-area neighborhoods.

IMAGE 2: Playground at Apodaca Park.  
IMAGE 3: Looking east down E. Madrid Avenue  
IMAGE 4: The Arroyo Trail at N. Triviz Drive



# Map I-A, Planning Area



Figure I.1, Focus Area A



Figure I.2, Focus Area B



## PLAN AREA DEMOGRAPHICS

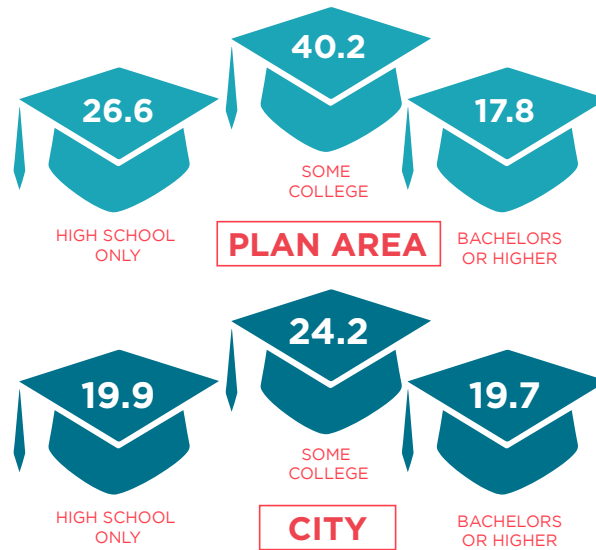
A quick snapshot of Planning Area demographics provides context to the general composition of the community the Blueprint will be serving. This information provides a basis for how Blueprint recommendations might affect segments of the Planning Area population.

**Figure I.3, Household Size<sup>1</sup>**



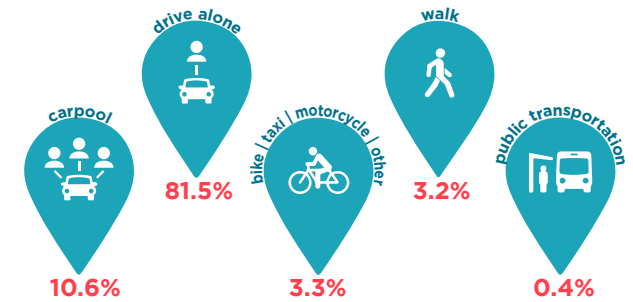
A majority of Planning Area households are comprised of three-plus members. Single-member households and two-member households are comparable in size, but together they make up just over 60% of plan area households. With this relative even mix in household size, one would expect a relative high variety of housing types.

**Figure I.4, Educational Attainment<sup>1</sup>**



Planning Area residents outperform the overall population of Las Cruces in terms of educational achievement. A large share of Apodaca area residents have some level college education - almost twice that of the City. Over 84 percent of the Planning Area population has at least a high school education, compared to 63.8 percent for the City as a whole.

**Figure I.5, Commuting Patterns**

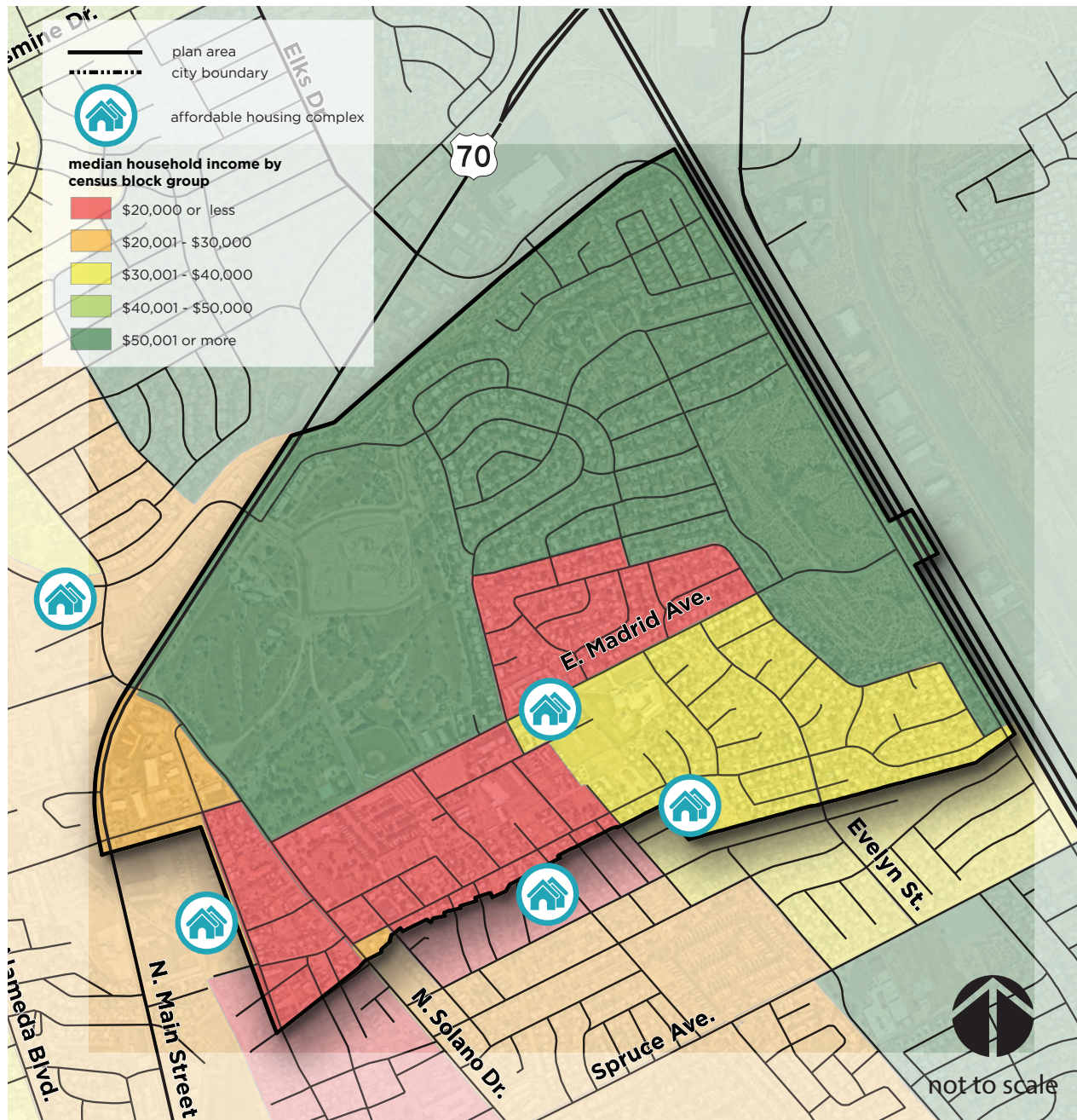


Planning Area residents' predominate mode of transportation for commuting to work is the personal automobile. Carpooling was the second highest method of commuting. Planning Area commuters were least likely to use public transit (0.4 percent) even though RoadRUNNER transit service is provided through Apodaca area neighborhoods.

1. Source: 2011-2015 American Community Survey (US Census)



## Map I-B, Median Household Income and Affordable Housing



### FINANCIAL CHARACTERISTICS

The information found in **Map I-B, Median Household Income and Affordable Housing**, highlights some of the socioeconomic characteristics of the Apodaca Planning Area. The median household income data from the US Census Bureau's 2011-2015 American Community Survey was mapped for Census block groups that overlap the Planning Area.

The median household income in the City of Las Cruces is \$41,330 dollars. In contrast to the Planning Area, there is only one Census block groups illustrated within **Map I.2** which illustrates a median household income that exceeds the city-wide figure.

Many Planning Area households earn incomes under \$20,000 dollars per year. To better understand the variance from the City median household income, the proximity of affordable housing complex was identified. These types of housing units often have income limits as a precondition for residency. It was determined that five of the 19 affordable housing complex are located within a Census block group that bisect the Apodaca Planning Area. The concentration of low-to-moderate income housing complexes helps account for at least a portion of the discrepancy between the City's and Planning Area's median household incomes.

1. Source: 2011-2015 American Community Survey (US Census), Mesilla Valley Public Housing Authority, Tierra Del Sol Housing Corporation



## Plan Area Conditions

An examination of land uses provides an understanding of the Planning Area’s built environment. The location, area, and relationship of land uses can provide insight on a specific neighborhood or district’s ability to handle growth and development, and where it should be focused.

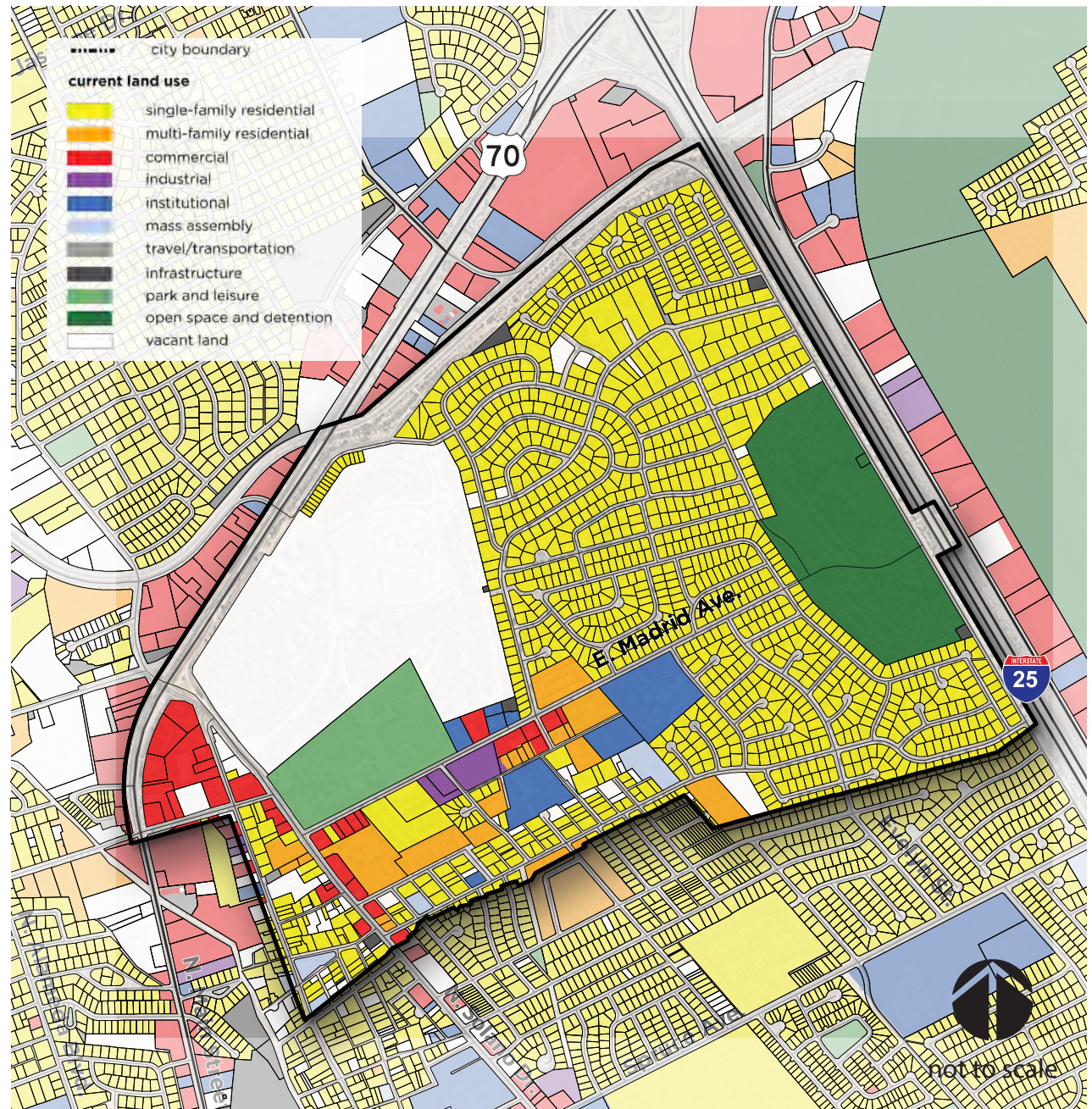
Single-family residential neighborhoods comprise the Apodaca Planning Area’s most common land use, accounting for almost 40 percent of the Planning Area. The second largest land use (excluding public right-of-way) is vacant land (including the Las Cruces County Club property). The single-family residential land use category identified within the Planning Area include different types of housing: detached residences, duplexes, mobile homes, and townhouses. Planning Area land uses become more intermingled as E. Madrid Avenue approaches N. Solano Drive, and as commercial and multi-family residential land uses become more common.

**Table I.1, Current Land Use Breakdown<sup>1</sup>**

Land Use	Acreage	%
Single-Family Residential	285.4	39.1
Multi-Family	27.3	3.7
Commercial	21.9	3.0
Industrial/Warehouse	4.5	0.6
Institutional	19.2	2.6
Mass Assembly	4.8	0.7
Infrastructure	2.2	0.3
Travel/Transportation	0.7	0.1
Park and Leisure	27.1	3.7
Open Space/Detention	56.0	7.7
Vacant Land	126.3	17.3
ROW (streets, drainage, etc.)	153.7	21.1

Source: City of Las Cruces

## Map I-C, Current Land Use





## PLAN AREA ISSUES

### PUBLIC TRANSPORTATION

RoadRUNNER Transit provides public bus services to the Planning Area on fixed routes. (Additional bus route expansion further into the plan area planned for the near future.) The general social and economic characteristics of Planning Area residents suggests that an affordable public transportation option would be a beneficial public service. As identified in **Figure I.5, Commuting Patterns**, (page 6) public transportation has a low utilization for commuting choice. There is an opportunity to capitalize on increased ridership within Apodaca area neighborhoods.



IMAGE 5: Existing bus stop on E. Madrid Avenue located in front of the Alta Tierra Apartment Complex. E. Madrid Avenue does not currently have a bus route, but there is a proposed bus route.

### MADRID AVENUE

Sidewalks, bicycle lanes, street parking, and wide travel lanes are present along the majority of E. Madrid Avenue. The street bottle necks at the E. Madrid Avenue and Martha Drive intersection, where the street parking, bicycle lanes, and sidewalks are discontinued for the remainder of the street as it approaches N. Triviz Drive. E. Madrid Avenue's street width, and relatively low traffic volumes, contribute to the ability for traffic to frequently exceed the posted speed limit.

Localized street flooding impacts property adjacent to the roadway during heavy rain events. Although E. Madrid Avenue is curbed and guttered, storm water flows within the street to an outlet at Loma Heights Elementary School and for the remainder of the street toward N. Solano Drive during heavy rain events.



IMAGE 6: The E. Madrid Avenue and Martha Drive intersection where residential lots directly adjacent to the road constrain the street right-of-way.

### BUILT ENVIRONMENT

The west portion of the Apodaca Planning Area is defined by a variety of land uses. This contributes to a lack of cohesion and uniformity from one property to another, leaving the built environment feeling and functioning in a disjointed manner. This issue is conflated by the gaps or breaks in the street grid that inhibit both vehicle and pedestrian circulation. The eastern portion of the Planning Area is a stark contrast, where fairly uniform single-family residential development creates a consistent character.

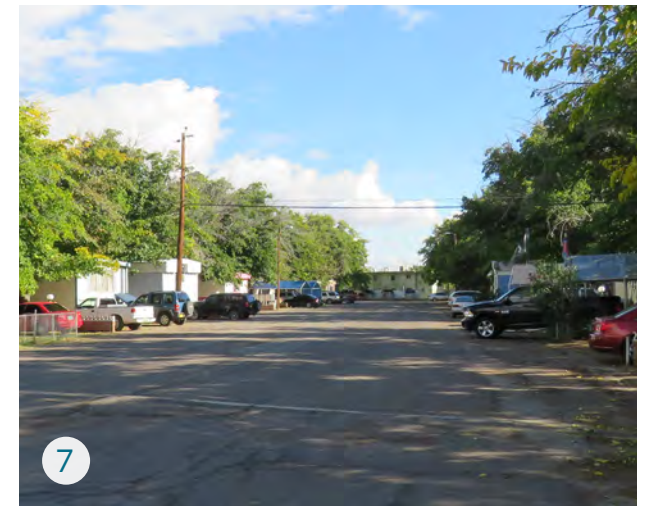


IMAGE 7: The Park South Mobile Village is an example of the variable land uses that are located in the western portion of the Planning Area.

## COMMUNITY PERCEPTIONS

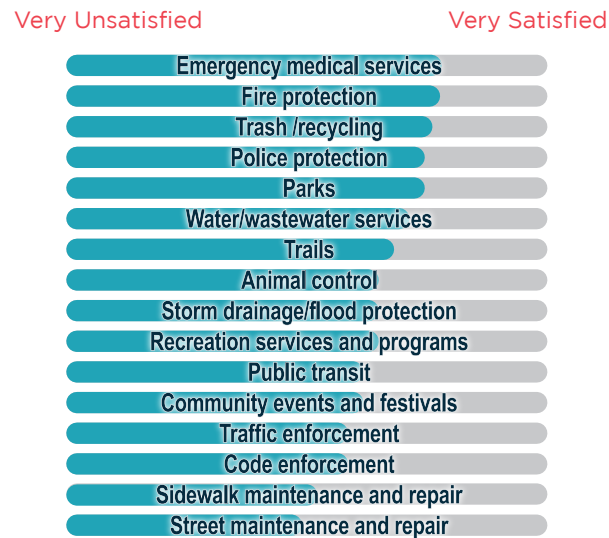
In planning it is often said that “perception is reality” and when approaching how a community views itself, that saying is especially true. As part of the assessment of Planning Area conditions, community members were asked to express their opinions on a multiple topics. **Figures I.6** through **I.8** summarize some of the responses gathered via on-line public outreach efforts. The responses help paint a picture as to how the community views Planning Area issues.

**Figure I.6, How has the plan area changed?**



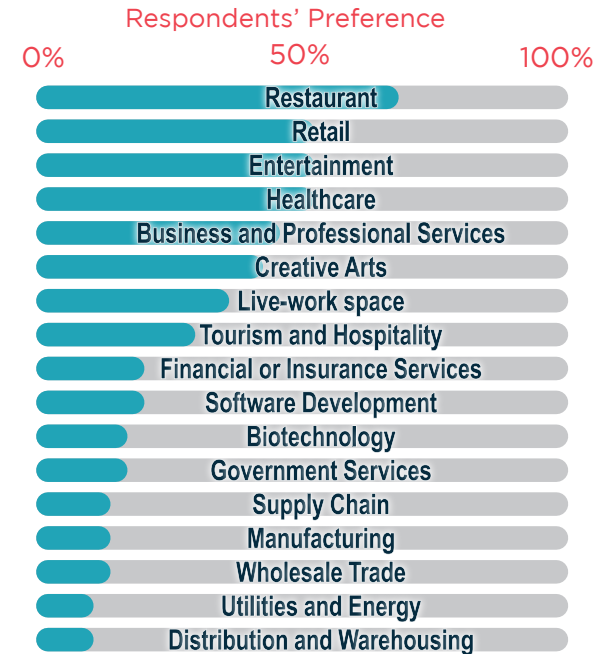
Survey respondents have overwhelmingly stated that they feel that the area has changed for the worst in recent years. Only 12 percent of the population think the Apodaca Planning Area has changed for the better, leaving the remaining 88 percent thinking that there has not been any noticeable improvements to the Planning Area.

**Figure I.7, Satisfaction with City services**



Survey respondents were asked to express their overall satisfaction with City services in the Planning Area. The results showed a generally satisfied community, with the least satisfaction towards City services being sidewalk and street maintenance. Code and traffic enforcement were the next most pressing needs targeted for improved service delivery.

**Figure I.8, What factors inhibit business growth/prosperity?**



Survey respondents had the opportunity to identify the types of business that they would like to see in the Planning Area. Respondents stated that restaurants are the most important commercial need in the Planning Area - followed by retail, entertainment, and healthcare. The least preferable business types are industrial in nature.



# Community Engagement

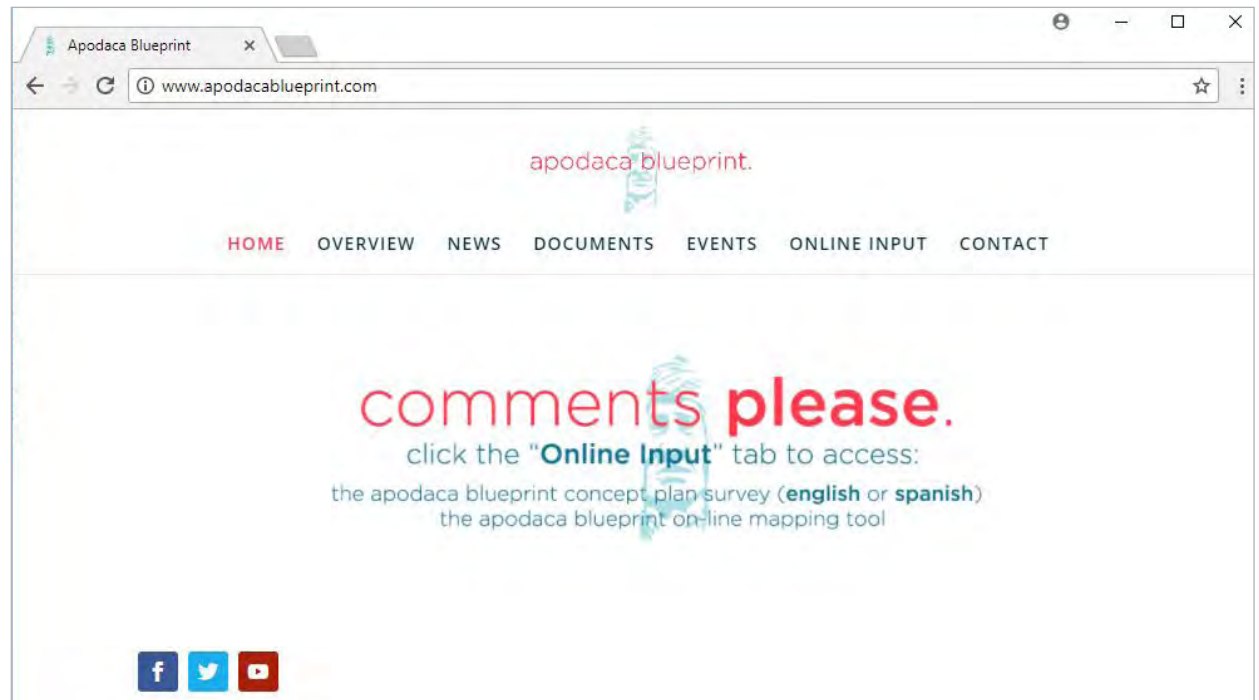
The role of public opinion in the Apodaca Blueprint planning process is paramount to the plan's long term success. The future of the Las Cruces Country Club property has had a very public history which has led to a concerted effort to increase the amount of public exposure of the planning process. Key community engagement activities included: **A)** A project website; **B)** On-line surveys; and, **C)** Social media blasts, as well as other approaches used to gather community involvement from a variety of stakeholder groups.

## STAKEHOLDER MEETINGS

The Blueprint consultant team met with public officials and various stakeholder groups in November 2017. Public officials provided context about the Planning Area, and hosted a City-led site tour. Other stakeholders represented various interests including Planning Area residents, business owners, service providers, community groups, real estate interest, as well as other concerned citizens.

## WEBSITE

The project website was a multifaceted platform used to inform the general public by hosting all project related news, documents, providing opportunities for public input, and highlighting the project time-line. The website was also used as a portal to collect responses from community members on proposed concept plans. An on-line mapping tool showcased the concepts and receive geographically referenced and site specific comments. The website was launched on November 8th, 2017 for public viewing and remained up during the duration of the planning process.



## COMMUNITY SURVEYS

Direct solicitation about planning area issues was conducted via two on-line surveys. The first survey asked participants questions about neighborhood conditions and changes or additions to the Planning Area. This survey was open for participation from November 27, 2017 through January 15, 2018.

The second survey focused on changes to the planning area's built environment. Participants were asked their opinions on development concepts. This information was used to refine the issues and ideas for Focus Areas A and B in public design workshop held in January 2018. The participation window for the second survey was from February 5, 2018 to March 13, 2018

These survey methods were helpful in reaching a wider audience who may not have been inclined or unable to attend one of the Blueprint public meetings. Over 244 individuals participated in the first survey, and 167 participated in the second.

## FOCUS AREA DESIGN WORKSHOP

A Focus Area Design Workshop was conducted, occurring January 8th through January 11th, 2018, where the consultant team hosted a variety of information gathering sessions. Workshop activities were open to all members of the public who chose to participate during any of the activities held over a four day period - including formal events and activities, as well as public drop-in hours for visitors to engage the consultant team directly.



*IMAGE 8: Community members discussing the vision for the planning area to City staff and the planning consultants.*

## **ON-LINE MAPPING TOOL**

An on-line map tool was used to receive direct comments from the public on proposed development concepts for Focus Areas A and B. Two different concepts for each focus area were developed at the workshop. These concepts were refined and geographically referenced into a web map for viewing, so that public comments were linked to specific components of the development concepts. The participants were able to classify these comments according to land use, transportation, open space, or miscellaneous. The on-line mapping tool was available between February 19, 2018 until March 13, 2018.

## **PUBLIC OPEN HOUSE**

A public open house was held on March 12, 2018 to receive comment on the two concepts that were developed for Focus Areas A and B. The open house format included a series of display boards that showcased potential future development concepts. Results from the second survey were included in the boards on display, and highlighting the public reaction to development concepts for the focus areas. Comment cards were provided to participants who wished to remark on the preliminary concept plans displayed.

The design workshop was advertised by a variety of methods in the weeks leading up to the event. A few of the outreach methods included fliers disseminated at the local elementary school, public announcements, signage posted in the plan area, and social media blasts.

A public open house was held at the beginning of the week to present the existing conditions information and to solicit specific comments on issues facing the planning area. Two public walking tours of Focus Area A were conducted so that residents could comment directly on the makeup of the Planning Area's built environment and exchange ideas with the consultant team on how to address associated issues.

A design exercise was also held where community members worked with the consultant team in developing a future vision for the Planning Area. This activity allowed participants to help illustrate the desired future for the types of land use offerings and built form of each Focus Area.

The workshop culminated in a public presentation of two illustrative development concepts for the planning area, one each for focus area A and B.



## **II. APODACA BLUEPRINT PROGRAM**

# Apodaca Blueprint Program

Implementation of the Apodaca Blueprint vision and objectives requires: **A)** Refined future land use and development policies for planning area properties; and, **B)** An action plan that includes clear strategies to promote planning area redevelopment and revitalization. The following Blueprint report components address these essential needs:

- **Future Development Maps.** Maps and corresponding narrative that illustrate proposed districts and corridors within the Apodaca planning area and focus areas according to preferred land use combinations, and building and site design characteristics.
- **Apodaca Blueprint Action Plan.** A series of actions – including proposed operational changes, policies, regulations, studies, and financial investments – that may facilitate the implementation of the future development maps and additional neighborhood conservation activities.

## APODACA PLANNING AREA FUTURE DEVELOPMENT MAP

The Apodaca Planning Area Future Development Map (facing page) establishes formal policy for future land use and development in the Apodaca planning area. The Future Development Map illustrates proposed districts and corridors which may develop or redevelop with varying combinations of land uses, and according to particular building and site design characteristics. In addition to other adopted municipal plans, these “character areas” should serve as a guide to City staff, and elected and appointed officials, when evaluating the merits of individual land use, development, and redevelopment proposals for property within the Apodaca planning area.

### FUTURE DEVELOPMENT MAP PARAMETERS

The Future Development Map depicts four character areas in the Apodaca Planning Area. Each are individually described in more detail on pages 16 and 17. The Las Cruces Country Club and Villa Mora focus areas (Focus areas A and B) are discussed in separate sections of the Blueprint beginning on pages 33 and 49, respectively.

Character area descriptions include the attributes highlighted in **Table II.1**. Unlike a parcel-specific future land use map, the boundaries of the Future Development Map’s character areas are conceptual and represent “approximate” location. Character area boundaries may also cross parcel lines. As a result, it may be assumed that small parcels located directly adjacent to, or split between, one or more character areas may be permitted to develop according to the parameters of the adjacent area.

### Character Area Attributes

Attributes	Details
<b>Description</b>	Briefly describes the preferred development type and intensity in the character area.
<b>Representative Land Uses</b>	General land uses that may be appropriate within the character area, subject to conditions. Lists are not all-inclusive; nor, meant to imply that each land use is appropriate in all parts of the character area.
<b>Recommended Development Patterns</b>	Preferred growth and development characteristics within each character area. Listed development patterns should be viewed as initial “core” recommendations which may be added to or otherwise modified.

*Halff Associates*

The applicability of character area boundaries is subject to the interpretation of the Mayor and City Council. Regardless, the City should initiate amendments to the Future Development Map whenever it intends to promote a development pattern contrary to the adopted map.

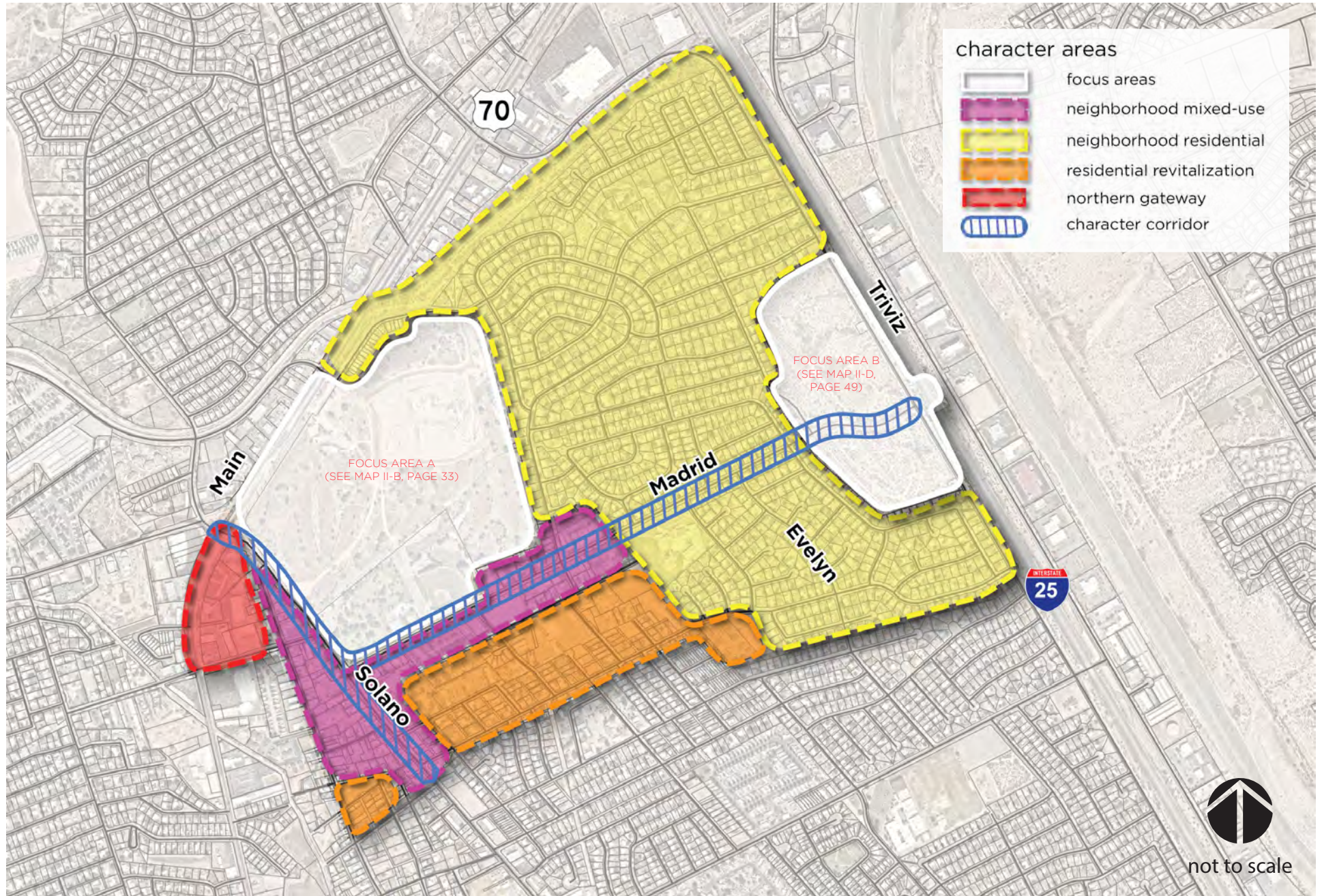
### PLAN4LASCRCUCES

The Apodaca Blueprint future development maps are an extension of the City’s adopted Future Concept Map (incorporated as part of the Sustainable Growth theme of Plan4LasCruces). Consistent with Goal 42 of Plan4LasCruces (and associated policies), the future development maps herein provide guidance regarding suitable land patterns within Apodaca Area neighborhoods.





# Map II-A, Apodaca Planning Area, Future Development Map





## NEIGHBORHOOD MIXED-USE



The Neighborhood Mixed-Use character area allows a mix of residential and non-residential uses. Building heights, massing, scale, and site arrangement, create transitions between high-intensity development on auto-urban corridors and surrounding residential neighborhoods. Mixed uses are spread among separate parcels or combined within buildings. Development frames adjacent streets and screens parking in a manner that promotes pedestrian comfort and sets it apart from typical large scale commercial or multi-family developments.

### REPRESENTATIVE LAND USES<sup>1</sup>

The following are representative land uses that may be suitable within the Neighborhood Mixed-Use character area when consistent with recommended development patterns:

- Multi-family Dwellings/Condominiums
- Townhomes
- Mixed-use Buildings (w/Ground Floor Commercial)
- Neighborhood Retail Services
- Restaurants
- Professional Offices
- Community Buildings
- Places of Worship
- Schools (K-12)

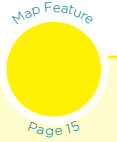
### RECOMMENDED DEVELOPMENT PATTERNS<sup>1</sup>

Development within the Neighborhood Mixed-Use character area should be guided by the following general parameters:

- The long side of buildings should frame the street with minimum frontage zones exceeding 40 percent or more of the lot width. Parking should not be permitted between the building and property lines adjacent to the street.
- Consistent build-to-lines should be established on collector and arterial streets to establish public/private frontage zones of sufficient width between the building and roadway for enhanced bicycle and pedestrian facilities or planting areas.
- Commercial and residential buildings should be oriented toward, and provide principal pedestrian access from, the street.
- Mixed-use buildings should be permitted by right on collector and arterial streets.
- Drive-thru and other auto oriented facilities should be limited to arterial roadways.
- Development scale - including building height and footprints - should provide a transition from adjacent residential uses.

<sup>1</sup> Not all-inclusive. Intended to establish a theme when considering rezonings, variances, conditional or special uses, or other development applications.

## NEIGHBORHOOD RESIDENTIAL



The Neighborhood Residential character area is reserved for single-family residential development and redevelopment. The character area may also support associated institutional and public uses such as places of worship, schools, parks, trails, etc. Although individual developments may exhibit common features including home size, lot size, setbacks, impervious surface coverage, etc., the character area allows variations of building and site features, subject to appropriate transitions in form, scale, and density between blocks or adjacent developments.

### REPRESENTATIVE LAND USES<sup>1</sup>

The following are representative land uses that may be suitable within the Neighborhood Residential character area when consistent with recommended development patterns:

- Single-family Residences (Detached)
- Accessory Uses and Structures
- Accessory Dwelling Units
- Home Occupations
- Community Buildings
- Places of Worship
- Schools (K-12)
- Parks

### RECOMMENDED DEVELOPMENT PATTERNS<sup>1</sup>

Development within the Neighborhood Residential character area should be guided by the following general parameters:

- A diverse mix of single-family detached housing types should be permitted, including variations in lot sizes, setbacks, and other spatial characteristics.
- Lot size should be deemphasized as the primary factor for distinguishing among residential development types. Focus on gross development density to promote variations in the housing market, and preserve open space.
- Transitions between residential development types should occur at intersecting streets and alleys rather than opposing block faces.
- Institutional uses such as places of assembly, schools, etc., should be sited so that vehicular access is taken from a collector street or arterial street.
- Street interconnectivity between developments should be prioritized.
- Active transportation facilities should be incorporated into new development and redevelopment activities.

## NORTHERN GATEWAY



The Northern Gateway character area supports local and regional businesses that rely on heavy traffic volumes and the associated visibility of being located near major roadways. Enhancements to future development such as improved landscaping, appropriately designed and scaled signage, bicycle and pedestrian accommodations, higher quality building materials, and access management techniques, should improve the auto-urban corridor's overall function, development quality, and appearance.

### REPRESENTATIVE LAND USES<sup>1</sup>

The following are representative land uses that may be suitable within the Northern Gateway character area when consistent with recommended development patterns:

- General Retail Sales
- Food Service Establishments
- Personal Service Establishments
- Professional Offices
- Shopping Centers
- Places of Assembly

### RECOMMENDED DEVELOPMENT PATTERNS<sup>1</sup>

Development within the Northern Gateway character area should be guided by the following general parameters:

- **Some buildings in retail centers and office parks - including out-parcels - should frame the street through minimum frontage requirements. Parking should not be permitted between the building and property lines adjacent to the street.**
- **Building form elements such as facade and roof-line articulation, access, fenestration, and variable building materials, should provide visual interest from the street.**
- **Building facades should be comprised of brick, stone, stucco or masonry construction along any exterior wall that faces a public right-of-way or principal parking area.**
- **Vehicular access should be managed by limiting the number and location of curb cuts, requiring cross-access easements (particularly for out-parcels) and providing minimum driveway throat lengths for motor vehicle queuing.**
- **Improve signage standards for new development or redevelopment in favor of monument signage rather than pole signage.**
- **Pedestrian facility connectivity should be required between the street, parking areas, public gathering spaces, and all buildings on the site.**

## RESIDENTIAL REVITALIZATION



The Residential Revitalization character area supports mixed residential densities and various housing types. Single-family detached and attached, multi-family, and institutional residential uses such as nursing homes/assisted living facilities may be permitted. Ancillary institutional/public uses such as places of worship, schools, parks, etc., may be permitted. Variation of residential form, scale, and density is allowed but there should be transitions between land uses, and encroachments into established single-family neighborhoods avoided.

### REPRESENTATIVE LAND USES<sup>1</sup>

The following are representative land uses that may be suitable within the Neighborhood Residential character area when consistent with recommended development patterns:

- Single-family Residences (Detached)
- Duplexes
- Townhomes
- Multi-family Dwellings
- Accessory Dwelling Units
- Home Occupations
- Community Buildings
- Places of Worship
- Schools (K-12)
- Parks

### RECOMMENDED DEVELOPMENT PATTERNS<sup>1</sup>

Development within the Neighborhood Residential character area should be guided by the following general parameters:

- **A diverse mix of housing types should be permitted, including variations in lot sizes, setbacks, and other spatial characteristics. Attached housing types of gradually higher density should be located closer to collector and arterial streets.**
- **Transitions between residential development types should occur at intersecting streets and alleys rather than opposing block faces.**
- **Residential uses should be designed with minimum masonry requirements and a high level of landscaping in parking lots and other common areas.**
- **Orient new multi-family and townhouse buildings toward existing street frontage, (even where remaining buildings face the interior of the development). Apply urban characteristics to street facing buildings including street-facing pedestrian access and internal stairwells and landings.**

<sup>1</sup> Not all-inclusive. Intended to establish a theme when considering rezonings, variances, conditional or special uses, or other development applications.



## REPRESENTATIVE IMAGES



IMAGE 12: Signage should be understated. Basic material standards should ensure a consistent image along thoroughfares.

IMAGE 13: Building form elements such as projections, roof lines, fenestration, and materials should vary across unified facades to provide visual interest.

IMAGES 9 & 10: Where minimum frontage requirements exist, the long edge of buildings should address the street.

IMAGE 11: Where possible, transitions between housing type and character should occur at intersection streets and alleys; although, such transitions may be incremental within existing blocks that are redeveloping.



## REPRESENTATIVE IMAGES



## CHARACTER CORRIDORS

E. Madrid Avenue and N. Solano Drive are important corridors within the Apodaca Blueprint planning area not only because of: **A)** Their role in distributing motor vehicle traffic; but also, **B)** The manner that they define surrounding neighborhoods and districts.

E. Madrid Avenue forms the Apodaca Planning Area’s “Main Street” - tying together a mix of residential areas, schools, parks, and neighborhood commercial establishments within a short distance. Yet paradoxically, the current design of the roadway does not project a vibrant image and can serve as a barrier between Apodaca area neighborhoods. Likewise, N. Solano Drive presents an inconsistent building pattern and site arrangement.

**In conjunction with the Apodaca Planning Area Future Development Map, recommendations on the design of these important neighborhood “character corridors” is provided beginning on page 25 of the Blueprint.**

## APODACA BLUEPRINT FOCUS AREAS

The Apodaca Blueprint Focus Areas include: **A)** The former Las Cruces Country Club property located on the south east corner of N. Main Street (U.S. Highway 70) and N. Solano Drive, and adjacent to Apodaca Park; and, **B)** The Villa Mora property which straddles the intersection of E. Madrid Avenue and Triviz Drive.

The future development character of both focus areas is not depicted on the Apodaca Planning Area Future Development Map. **Land use and development guidance on these focused sites is provided within separate sections of this Blueprint beginning on pages 33 and 49, respectively.**



*IMAGE 14: Bicycle and pedestrian infrastructure should be incorporated into new or retro-fitted street segments as directed by the City’s active transportation plans.*  
*IMAGE 15: Land development regulations should include on-site connectivity provisions to provide for defined pedestrian pathways between the street right-of-way and on-site buildings and parking areas.*

# Apodaca Blueprint Street System

In addition to the character areas introduced on pages 16 through 17, the design of the Apodaca Planning Area street system plays an equivalent role in defining the preferred form and function of new development. This contributive relationship between the public street and private property is acknowledged through the concept of the “transportation/land use connection” – whereby the arrangement of the roadway elements within the street right-of-way directly influences the style and intensity of development on abutting properties.

This section of the Blueprint defines recommended street system characteristics for the Apodaca Planning Area including the following:

- **Street Network.** Suggests adjustments to the arrangement of Planning Area streets to ensure an interconnected network and blocks of a walkable scale. Considers street closures, re-alignments, and additions; and, areas where an “urban” street context should be considered.
- **Street Types.** Introduces alternative street sections within a recommended urban context zone.

The Blueprint’s recommended street system characteristics should be considered in conjunction with the Apodaca Planning Area Future Development Map (page 33) when evaluating the merits of individual land use, development, and redevelopment proposals for property within the Planning Area. In addition to policy considerations however, the City of Las Cruces can (and should) play a more pro-active role in ensuring that changes to the public street system reflect community design preferences. Street system implementation strategies are contained in the Apodaca Blueprint Action Plan beginning on page 62.

## STREET NETWORK PARAMETERS

**Figure II.1**, Planning Area Street Network (facing page), depicts potential refinements to the arrangement of existing and future streets in the Apodaca Planning Area. The figure has been prepared to conform with the Mesilla Valley MPO’s Future Thoroughfare Map, but presents a more detailed Planning Area arrangement of major thoroughfares and local streets. A description of the principal features that are illustrated in **Figure II.1**, is provided in **Table II.2, Street Network Attributes**.

**The street network elements depicted in Figure II.1 are not regulatory. They are conceptual and are intended only as a guide.** The final arrangement of street network elements (and design of transportation facilities) will vary depending on future development proposals, revisions to Las Cruces Development Code, and the allocation of capital funding.

**Table II.2, Street System Attributes<sup>1</sup>**

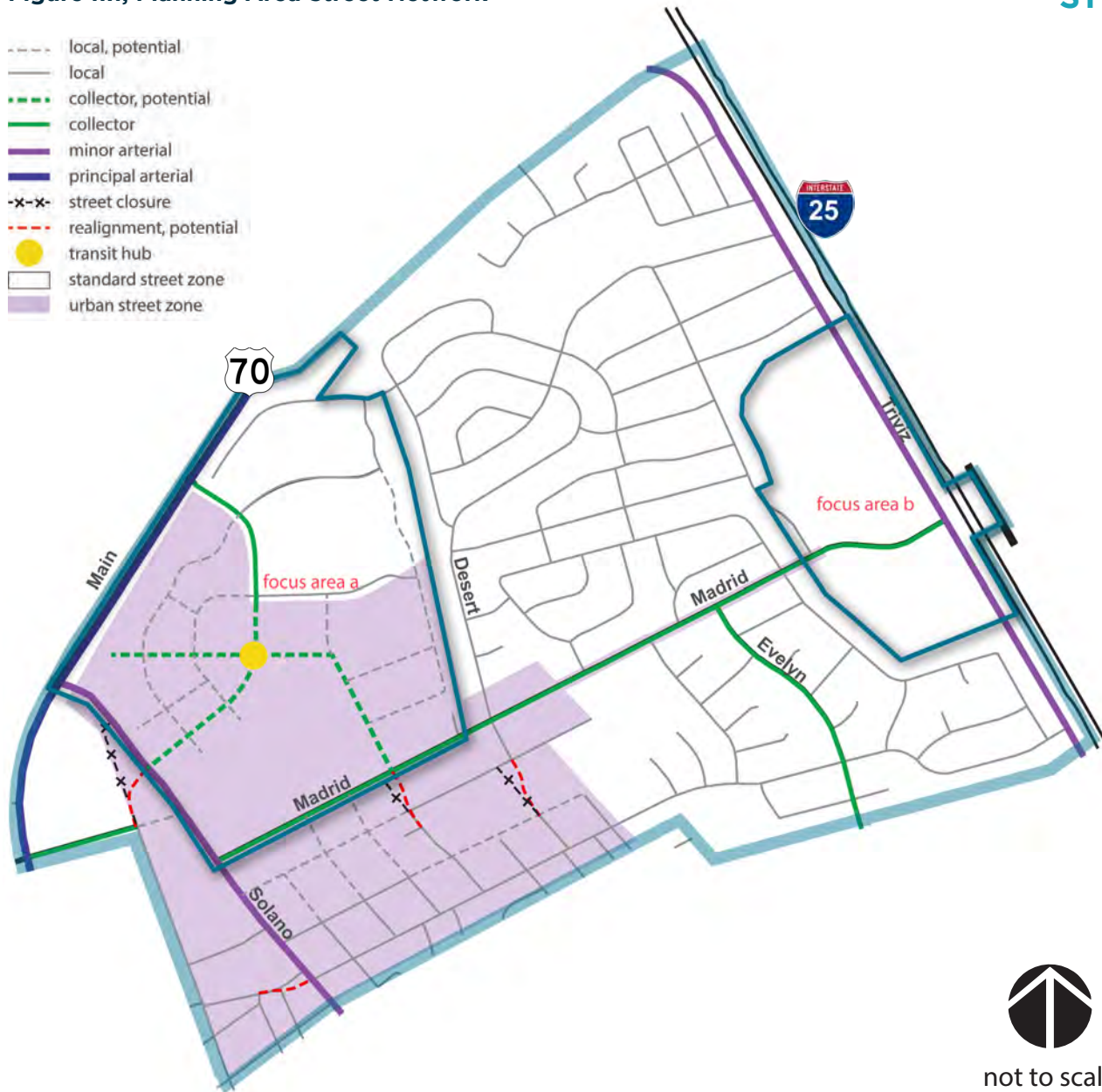
Attributes	Details
<b>Thoroughfare Classification</b>	Existing and potential streets identified according to functional classification.
<b>Street Reconfigurations</b>	Possible street additions, re-alignments, and closures intended to improve street grid scale and interconnectivity.
<b>Street Character Zones</b>	Areas where differing street standards may be applied to reflect an existing or intended development pattern. These context zones should be considered in conjunction with surrounding character areas and may be extended beyond the boundaries of the Planning Area <sup>2</sup> .
<b>Transit Hub</b>	The approximate location of an “enhanced” transit stop to support the development of a new employment and activity node <sup>3</sup> .

1. Depicted in Figure II.1, Planning Area Street Network.

2. “Character areas” are defined in conjunction with Map II-A, Apodaca Planning Area, Future Development Map.

3. “Enhanced” transit stop described on page 58.

**Figure II.1, Planning Area Street Network**



## STREET SYSTEM DESIGN PRINCIPLES

The Apodaca Planning Area’s conceptual street network presumes the division of the planning area into two street character zones. Both zones promote distinct design relationships between roadway elements within street rights-of-way, and building and site features on adjacent properties. The street character zones are defined as follows:

- **Standard Street Character Zone.** Streets within this zone may be developed according to the provisions contained in the Las Cruces Development Code existing at the time of Blueprint adoption, or as amended.
- **Urban Street Character Zone.** Streets within this zone should be developed to balance the mobility needs of motorized and non-motorized users. Urban streets should be designed to provide a comfortable and aesthetically pleasing built environment that encourages bicyclist and pedestrian activity.

The Las Cruces Development Code already provides the regulatory language necessary to construct a roadway network that may be suitable in the standard street character zone. As a result, the street system design principles described in this section of the Blueprint are primarily intended to apply to roadway network development in the urban street character zone<sup>1</sup>.

## MISCELLANEOUS DESIGN PRINCIPLES

Additional design principles relating to mobility, including active transportation and public transit, are discussed within the “Planning Area Design Features” section beginning on page ??.



not to scale

<sup>1</sup>The application of Blueprint street system design principles outside of the urban street character zone may be suitable on a case-by-case basis at the discretion of City officials.

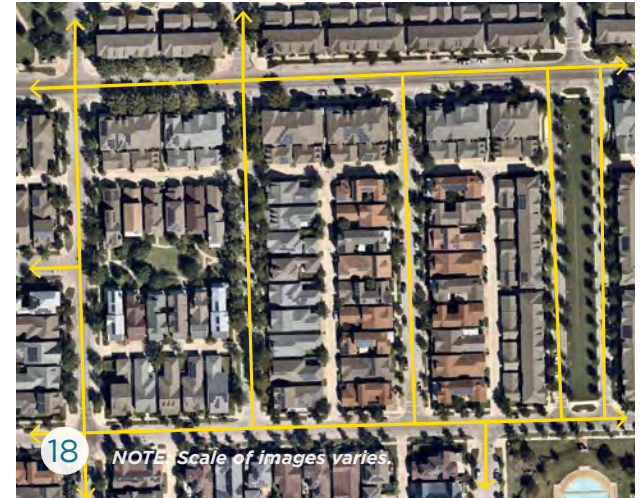
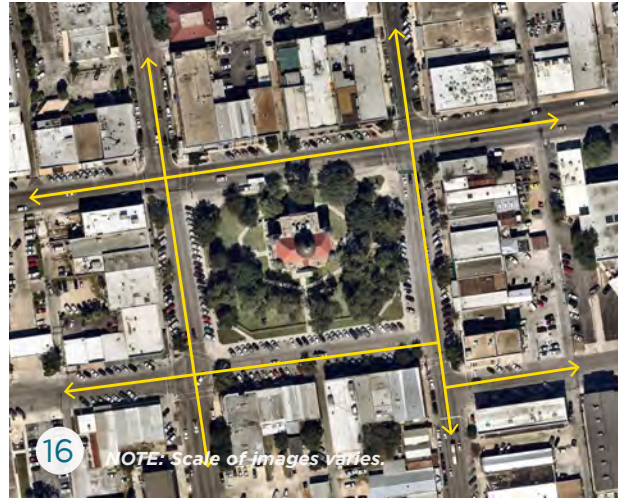


## STREET NETWORK

Urban street networks are designed to promote compact development and encourage pedestrian travel. These networks are arranged to efficiently disperse motor vehicle traffic while maximizing pedestrian access between multiple destinations within a five to ten minute “pedestrian shed” (typically between a 1/4 and 1/2 mile radius) and incentivizing pedestrian activity. In short, street interconnectivity should be valued and maximized within urban street networks.

## STREET CONNECTIVITY

A variety of measures are used to evaluate a community’s street connectivity characteristics, or to establish local goals including: link-node ratio, intersection density, path density, and block size. Among planning and engineering practitioners, block size is the most frequently used measure in establishing regulatory language related to street connectivity. Block size may in turn be measured by perimeter area, width, depth, section, and density per square mile. Assuming a general community goal of establishing a five-minute pedestrian shed, maximum block perimeters should not exceed 1320 linear feet.



*IMAGE 16: Many of the traditional courthouse square towns throughout the American Midwest and South developed according to near proportional grids with block lengths and depths ranging between 300 feet and 400 feet.*

*IMAGE 17: Mesilla's street grid frames blocks with perimeters of less than 900 feet.*

*IMAGE 18: Elongated grids can maintain compact block sizes and effectively distribute local traffic while also discouraging cut-through traffic due to the frequency of terminal intersections at mid-block locations.*

*IMAGE 19: Modified grids use subtle changes in the angle of streets at intersections and mid-block locations to create visual interest and slow traffic speeds.*

Block size is the principal measure guiding future street arrangement recommendations within the Apodaca Planning Area’s urban street character zone. As depicted by **Figure II.1**, (page 21) the street network within the Planning Area – and the urban street character zone in particular – should be arranged according to the following guidelines:

- Maximum block perimeter of mixed use and non-residential developments should not exceed 2000 linear feet.
- Maximum block perimeter of residential developments should not exceed 1320 linear feet.
- Block lengths that exceed 600 feet should be bisected by pedestrian pathways at or near the mid-point of the block face (and should align with intersecting streets where possible).
- New development should be arranged so that streets and pedestrian pathways may be extended to create blocks that meet the maximum perimeter recommendations herein.

## INTERSECTIONS

Although frequent street interconnectivity promotes the distribution of motor vehicle traffic, the efficiency and safety of the street grid may be inhibited by intersecting streets that are misaligned. Odd street offsets can create conflicting and unsafe turning motions onto and across bisecting roadways. Correcting these offsets can be cost prohibitive because of the existence of other utilities within the street right-of-way.



*IMAGE 20: Pedestrian pathways should link streets at mid-block locations where the block length exceeds 600 feet.*

**Figure II.1** (page 21) illustrates some locations within the Apodaca Planning Area where street realignments may be necessary over time to facilitate greater street grid interconnectivity. Participation by the City of Las Cruces may be necessary to complete these realignments.

## EXCEPTIONS

Within the Apodaca Planning Area, there may be exceptions to the street network recommendations promoted herein. Exceptions may include:

- **Apodaca Park.** The Blueprint recommends consolidating existing park land and limiting traffic to the perimeter of Apodaca Park. The consolidated size of Apodaca Park may require a larger perimeter block length to avoid promoting through-traffic.

- **Existing Thoroughfare Network.** Existing thoroughfare characteristics and traffic volumes (particularly on N. Man Street/ U.S. 70) may require greater spacing of intersecting streets. In such instances, block size recommendations may be met through the provision of appropriately spaced limited access driveways or streets. Access to and from these intervening driveways/streets may be limited to right-hand turning motions only.
- **Consolidated Development Tracts.** Large development tracts for land uses such as shopping centers or office parks often inhibit the extension of the public street system. Within the Planning Area, the City may opt to require driveway access through the development tract via private drives that align with public streets and controlled intersections. Driveways may be spaced to adhere to the block size recommendations herein, and may be converted into public streets at a future date.
- **Natural Features/Public Facilities.** The City may determine that site topography interferes with strict adherence to the Blueprint’s block size recommendations. Likewise, require public facilities such as detention ponds may require street extensions that exceed recommended block sizes, but are necessary to ensure overall interconnectivity.



## STREET TYPES

New and reconfigured roadways within the Urban Street Character Zone should incorporate context sensitive design features that exhibit the following three attributes: Complete Streets, Calm Streets, and Active Roadways. Each of these attributes is defined in **Figure II.2, Urban Street Conditions**.

Ideally, a community's land development regulations will include standardized design features that serve all user groups, calm traffic speeds, and generate active roadsides regardless of location. Within areas that are specifically designated for urban or traditional neighborhood development patterns however, street design features **must** collectively promote all three essential conditions.

**Figure II.2, Urban Street Conditions**

1

**COMPLETE STREETS.** Clearly defined accommodations are provided within the street right-of-way for motorized and non-motorized activity. The inclusion of sidewalks, designated bicycle facilities, and transit accommodations in new and reconfigured streets should be given the same level of importance as motor vehicle travel lanes.

2

**CALM STREETS.** Design features reduce the variance between a street's posted speed and design speed. Reduced travel lane widths and curve radii are combined with street scape elements and traffic calming features to manage motor vehicle speeds and volumes by constraining the physical and psychological scale of the roadway.

3

**ACTIVE ROADSIDES.** Street right-of-way networks collectively form a community's largest public space. Outside of the traveled way, well-designed street roadsides provide substantial opportunities for public gathering and activity.



*IMAGE 21: A "complete street's" inclusion of bicycle and pedestrian facilities does not guarantee that a roadway will be a comfortable place for active transportation. IMAGE 22: Calm streets are able to combine active transportation facilities with roadway features that moderate traffic speed on major thoroughfares. IMAGE 23: The combination of complete street and calm street features should create a roadside environment that combines bicycle and pedestrian travel with other leisure activities.*



## URBAN STREET TEMPLATES

Creation of an Urban Streets Character Zone within the Apodaca Planning Area and other targeted locations throughout Las Cruces requires the formal adoption of urban street standards as an alternative within the Las Cruces Development Code. **Table II.3, Urban Street/Functional Classification Relationship**, identifies three tiers of urban streets that can be developed to serve the same traffic flow functions as the Federal Highway Administration’s and Mesilla Valley MPO’s classifications for major thoroughfares.

The urban street types identified in **Table II.3** mimic the categories established within the Institute for Transportation Engineer’s, *Designing Walkable Urban Thoroughfares, a Context Sensitive Approach* (2010). Many variations of these urban street types can be created to serve different mixes of land use and development intensity.

Four urban street templates are illustrated on pages 26 and 27. These templates may be used by the City to generate a series of urban streets that can be incorporated into the Las Cruces Development Code.

**Table II.3, Urban Street/Functional Classification Relationship**

Urban Street Type	Functional Classification of Roadway <sup>1</sup>			
	Principal Arterial	Minor Arterial	Collector	Local
Boulevard	●	●		
Avenue		●	●	
Street			●	●

1. Mesilla Valley MPO, *Functional Classification Map (Transport 2040)*

The urban street templates are divided into two principal zones:

- **Roadway.** Includes motor vehicle travel lanes and adjacent elements that control or influence motor vehicle movements such as medians, parking lanes, designated bicycle facilities (located within the pavement section), and curb and gutters.
- **Roadside.** Includes zones outside of the roadway edge that support bicycle and pedestrian activity including furnishing zones, sidewalks, and accessory zones between the primary pedestrian pathway and building frontages.

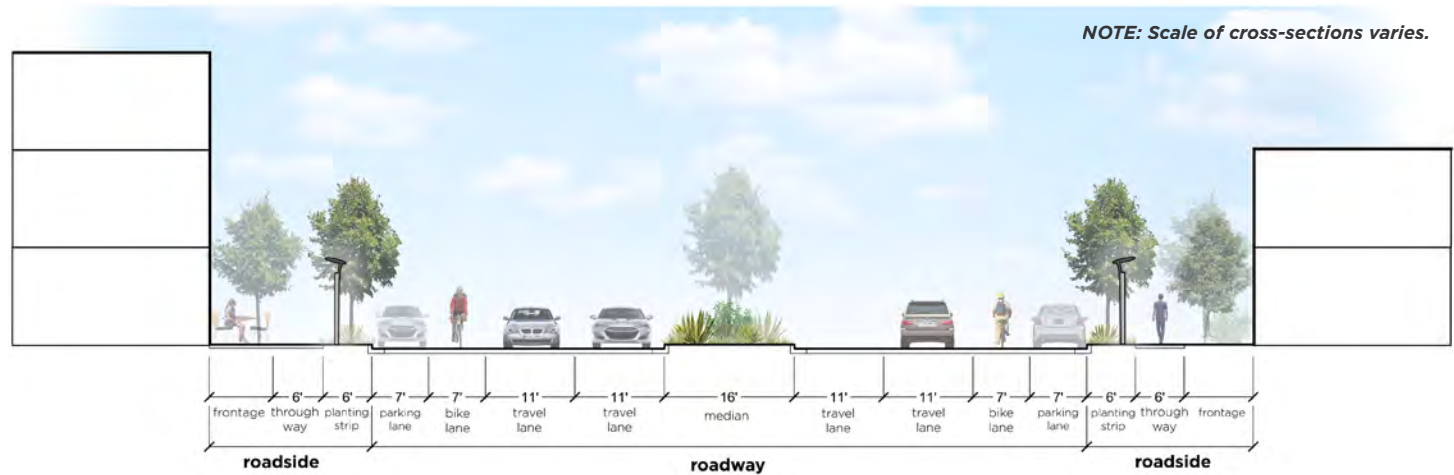
*IMAGE 24: Multiple configurations of roadway and roadside elements may be utilized to transform urban thoroughfares into corridors that not only provide a transportation function, but also serve as active destinations.*



The urban street templates introduced in the Blueprint represent only a handful of many different roadway and roadside combinations. City-initiated development of urban street types should not be limited to the designs illustrated herein. Regardless, each of the Blueprint’s four urban street templates have been incorporated into the Focus Area A conceptual site plan that is described beginning on page 39 Their inclusion within the Focus Area A conceptual site plan corresponds to the limits of the Urban Street Character Zone as depicted within **Figure II.1, Planning Area Street Network**.

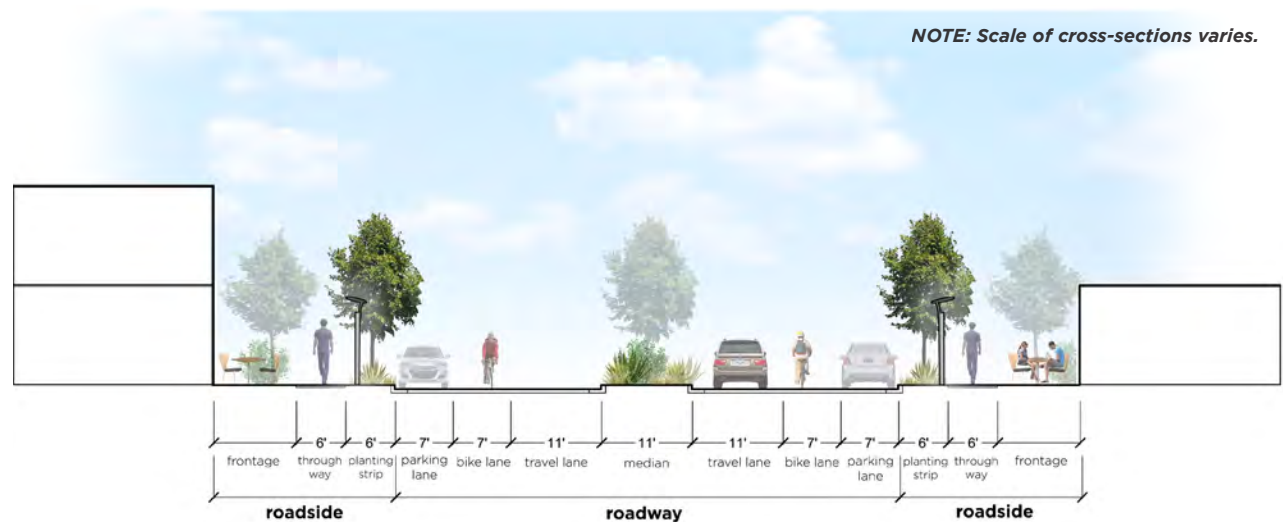
**Figure II.3, Boulevard**

The Boulevard depicted in **Figure II.3** allows for the highest volume of motor vehicle travel within an urban environment (over 15,000 AADT). More than any other urban street type, the Boulevard provides a through-way for motor vehicle traffic (in addition to access). It is distinct from typical arterial thoroughfares in that on-street parking and generous roadside widths shield pedestrian activity from motor vehicle movements. In addition, the heights of adjacent buildings provides a more balanced height/width ratio to add to an enhanced pedestrian environment.

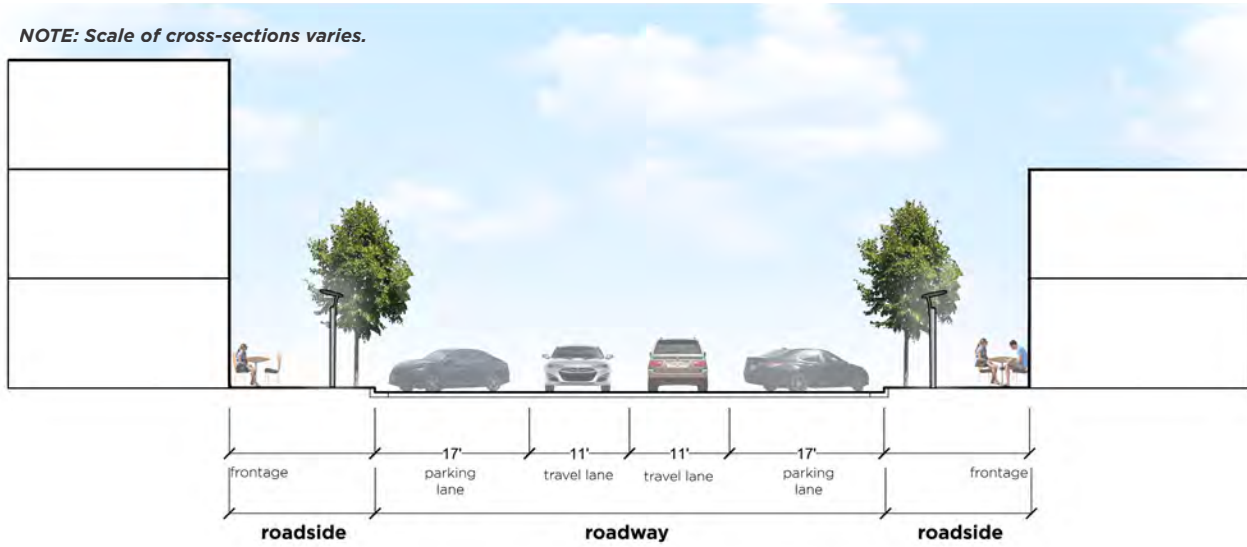


**Figure II.4, Avenue**

The Avenue depicted in **Figure II.4** shows two travel lanes, two bicycle lanes, two parking lanes, and a central median. The roadside includes defined planting strips, pedestrian through-way, and frontage area. The arrangement of roadside spaces may vary depending on the planting treatment chosen and desire for public gathering space. The extra-wide bicycle lane depicted accounts for the adjacent door zone due to parked cars and may be striped as a buffered lane. As an alternative, separated bicycle lanes may be located between the parking lane and planting strip. Anticipated to be utilized in higher intensity mixed use development areas, additional building height on one or both sides of the street will add to an enhanced pedestrian environment.

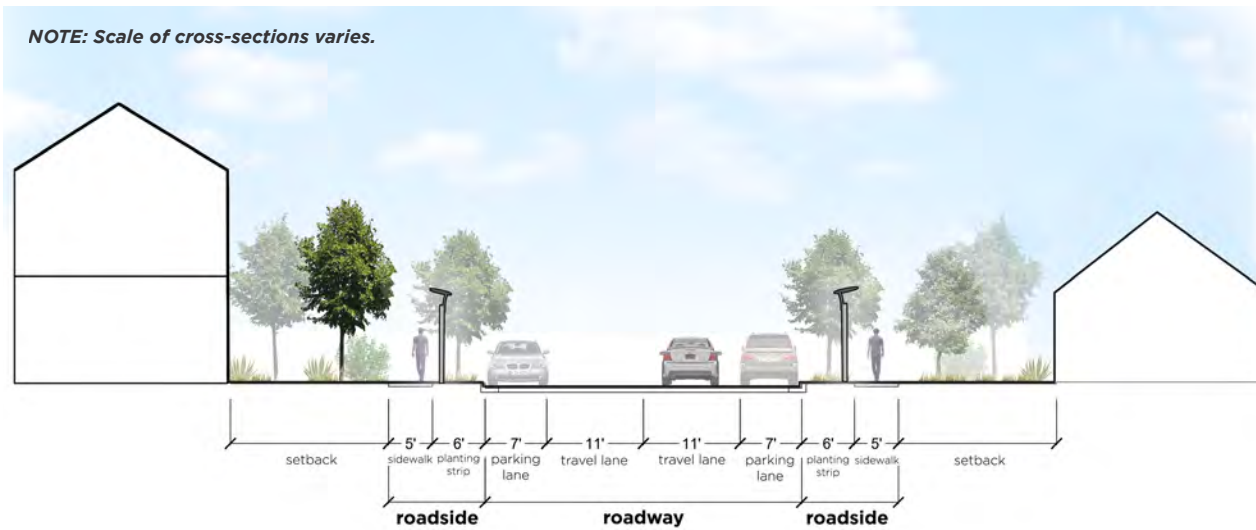


**Figure II.5, Street (Urban)**



The (Urban) Street depicted in **Figure II.5** includes two motor vehicle travel lanes and two angled parking lanes. The roadside illustrates an integrated frontage area where the pedestrian pathway merges with the planting area utilizing tree wells, and gathering areas. Designated bicycle facilities may not be necessary due to low estimated traffic volumes, but could be incorporated into the street section by replacing angled parking with parallel parking. Urban Streets are suitable in mixed use environments or in multi-story residential areas. As with the “Boulevard” and “Avenue” street cross-sections, additional building height on one or both sides of the street also adds to an increasingly comfortable pedestrian environment.

**Figure II.6, Street (Residential)**



The (Residential) Street depicted in **Figure II.6** includes two motor vehicle travel lanes and two parallel parking lanes. The roadside illustrates a planting strip, five foot sidewalk, and adjacent front yards. Along street segments connecting areas of higher development intensity, sidewalks should be widened to six to eight feet. As with the (Urban) Street, designated bicycle facilities may not be necessary due to low estimated traffic volumes, but could be incorporated into the street section by removing a lane of parking or providing additional right-of-way. Due to lower estimated development intensity on (Residential) Streets, single-story buildings are appropriate but two-to-three story residences can also provide a complimentary street edge.



## CHARACTER CORRIDORS

The E. Madrid Avenue and N. Solano Drive corridors are identified on the Apodaca Planning Area Future Development Map (page 15) as “character corridors.” Character corridors are not only important for their transportation function, but also for the manner in which they define surrounding neighborhoods and districts.

### DEVELOPMENT PARAMETERS

To support the development parameters of Planning Area character districts, at least some of the urban street design elements introduced on preceding pages should be incorporated into the E. Madrid Avenue and N. Solano Drive corridors. Enhancements may occur incrementally - in conjunction with new development on individual lots - or as part of a capital street improvement project.

Key development parameters may include:

- **Driveways.** Driveways should be consolidated in favor of cross-access easements to reduce curb cuts. Shared drives should be located to the rear of lots to allow for sufficient vehicle queuing.
- **Building Frontages.** No less than 40 percent of the lot width along Madrid Avenue (except for single-family residential areas) should be bordered by building facade.
- **Build-to-Lines.** Build-to-lines should establish public/private frontage zones of sufficient width between the building and roadway for enhanced bicycle/pedestrian facilities or planting areas.
- **Calm Streets.** Medians, curb extensions, and other traffic calming features should be added to E. Madrid Avenue to slow traffic. Intermittent medians may be added to N. Solano Drive to beautify the corridor and manage access.



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IMAGES 25 & 26: Current traffic volumes on N. Solano Drive are less than 15,000 AADT - making it a candidate for reconfiguration and possible lane reduction (25). E. Madrid Avenue meets the definition of a “complete street,” but should incorporate “calm street” enhancements.

- **Pedestrian Pathways.** Sidewalks should be setback from the back of curb by at least 6 to 12 feet where possible (typically in conjunction with new construction). Minimum sidewalk widths should not be less than six feet on Madrid Avenue and eight feet on Solano Drive.
- **Aesthetics.** Utilities should be buried as funding allows. Uniform signage standards should be developed with a particular emphasis on monument style free-standing signage.
- **Parking.** Parking should be located to the side and rear of buildings and not extend between the building frontage and street right-of-way.



27



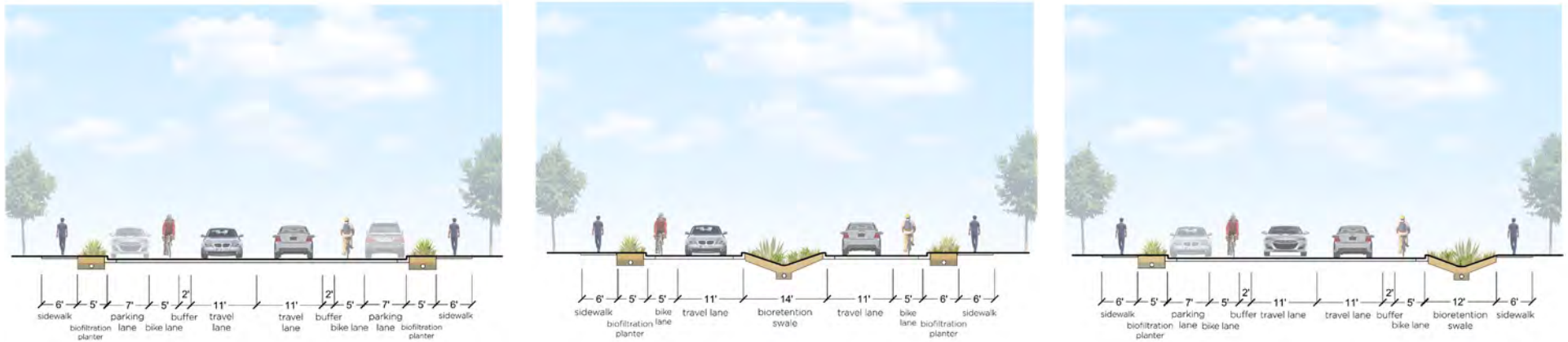
28



29

IMAGES 27, 28, & 29: Intermittent medians within the center turn lane of Avenida de la Mesilla balance access, aesthetics, and traffic calming (27). New development along University Drive forms a nice street edge, but insufficient roadside widths can make pedestrian activity uncomfortable (28). Local ordinances can require that developers provide wider roadside and enhanced pedestrian facilities as part of new development on established corridors (29).

**Figure II.7, Street, Madrid Avenue Cross-Sections**



**FIGURE II.7:** E. Madrid Avenue is the Apodaca Planning Area’s “Main Street.” The potential incorporation of “calm street” features to moderate traffic speeds on the street also provides an opportunity to apply green street technologies. The **Figure II.7** images above illustrate a series of green street features that can be combined with E. Madrid Avenue streetscape elements. Sequentially applied on adjacent segments of the street, the cross-sections can narrow motor vehicle travel lanes and create a series of diversions that require motorists to slow as they navigate the street.

**Figure II.8, Street, Madrid Avenue Reconfiguration**



**FIGURE II.8:** The plan view of E. Madrid Avenue depicted above provides an example of how many of the streetscape enhancements depicted in **Figure II.8** could be applied within a reconfigured corridor (as well as other optional traffic calming features). The figure also depicts possible building arrangements and roadway re-alignments that are consistent with the character area and character corridor development parameters (policies) that are recommended within other sections of the Blueprint.



## Focus Area A: Las Cruces Country Club

The majority of Focus Area A is comprised of the former Las Cruces Country Club property. Since the closure of the country club and golf club in late 2011, proposals have been submitted to the City to develop a mix of commercial and residential uses on the site. Some of these proposals have experienced opposition, and development activity on the site has

been limited due to a related lawsuit. As a result, only a portion of the property has been rezoned to allow for commercial and multi-family use; and, only limited infrastructure has been constructed. These previous experiences – including reservations by some surrounding residents – directly influenced the City’s decision to commission the Apodaca Blueprint.

### SITE FEATURES

**Figure II.9** (facing page) illustrates significant site features that have been observed during the Blueprint planning process. These features influence the future land use and development patterns recommended within Focus Area A.

Some key features include:

- **Apodaca Park.** Site features and programming can be adjusted to provide the park with better access and organization, and relationship to surrounding development (See page 43).
- **Dineh Statue.** Although located at a prominent street intersection, this iconic City feature is detached from most activity within the park (and given the distractions of drivers, may be hiding in plain sight).
- **Country Club Building.** Portions of the former Country Club building may be incorporated into a developed site as an event venue, restaurant, or similar public gathering space.
- **Drainage.** The existing drainage area in Apodaca Park may be expanded/reconfigured to address regional storm water detention needs. Detention areas may be designed to provide additional flex field space.

- **Madrid Avenue Access.** Potential motor vehicle access to the site from E. Madrid Avenue is limited due to the configuration of Apodaca Park and park facilities.
- **Parking.** Existing parking in Apodaca Park bisects the property and disrupts the flow of existing and potential recreation spaces.
- **Outfall Channel Trail.** A potential linkage to the existing trail provides an extension of the city-wide trail system, and enhances its utility as an active transportation corridor.
- **Solano Drive Access.** An existing utility easement and park access drive from N. Solano Drive provide access options, but also challenges due to intersection spacing.
- **Three Crosses Park.** An enhanced memorial may be incorporated into new development.
- **Utility Easement.** The overhead power lines bisecting the eastern side of the site delineate a potential buffer area for open space or low-intensity development abutting adjacent neighborhoods.
- **Vistas.** With only slight changes in elevation – whether through buildings or earthen features – site development may take advantage of views of the surrounding Organ Mountains National Monument, and other mountain peaks.



*IMAGE 30: The Las Cruces Country Club clubhouse building was designed by Henry C. Trost - an architect of regional significance. Incorporating the structure into future development scenarios provides a link to the site’s past.*





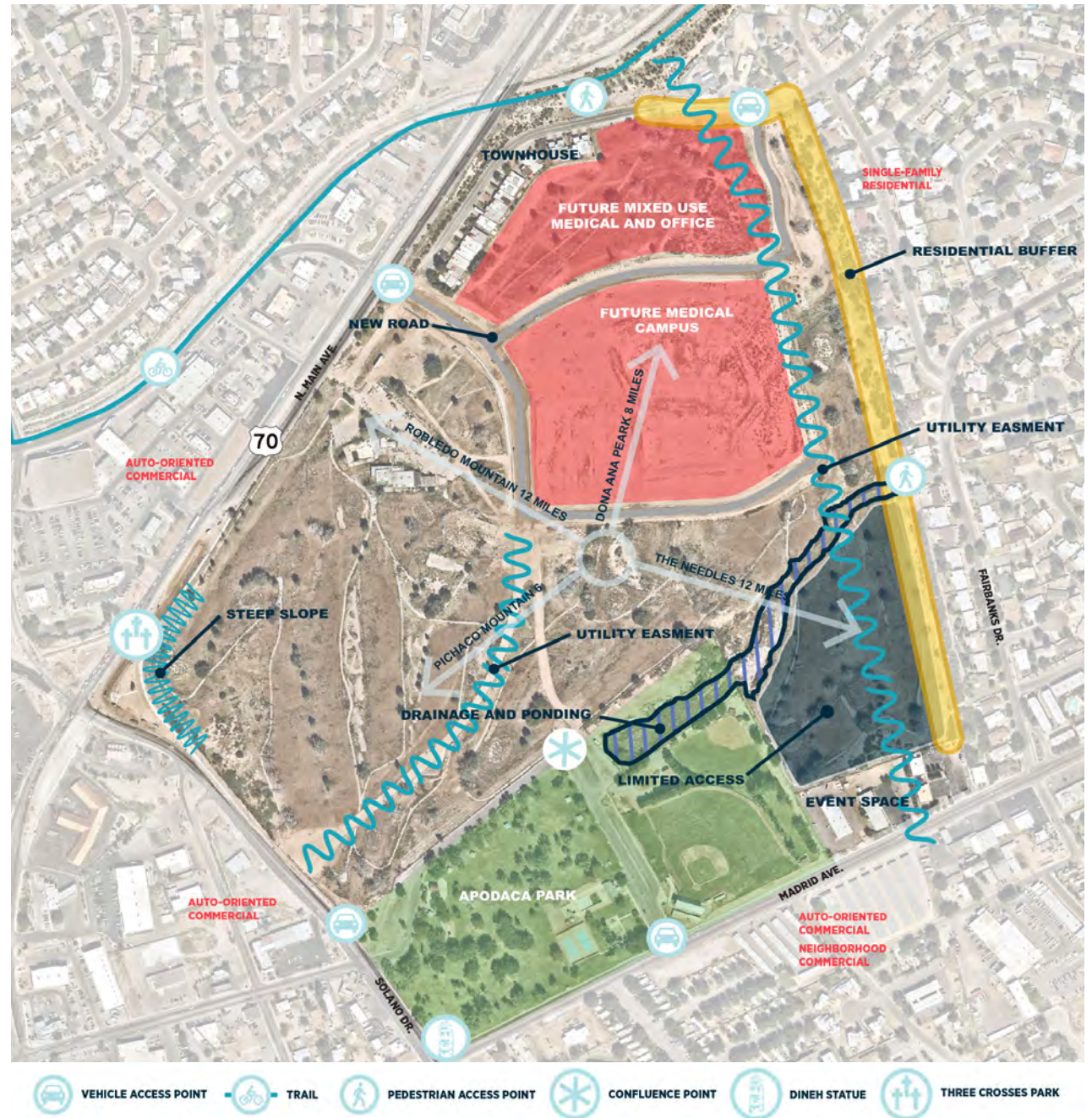
31



32

IMAGE 31: Existing City property adjacent to Apodaca Park could provide regional detention options to offset new development.  
 IMAGE 32: New street infrastructure and the existing power lines running north to south delineate a narrow strip of land on the eastern boundary of Focus Area A.

FIGURE II.9, Focus Area A Site Features



## Focus Area A, Future Development Plan

The Apodaca Blueprint planning process generated a future development plan for Focus Area A that is more detailed than that proposed for other portions of the Blueprint Planning Area. The Focus Area A Future Development

Map (**Map II-B**, facing page) depicts nine “character areas” which establish basic land use and development characteristics for focus area properties. These character areas are described in more detail on pages 34 through 36.

### INTERPRETING THE FUTURE DEVELOPMENT PLAN

As with the Apodaca Planning Area Future Development Map (page 15), the “character areas” depicted by the Focus Area A Future Development Map serve as a guide to City staff, and elected and appointed officials, when evaluating the merits of land use and development proposals for focus area property. When utilizing the Future Development Map and corresponding character area descriptions, municipal decision-makers are encouraged to exercise discretion on a case-by-case basis.

The following assumptions should be made when referencing the Focus Area A Future Development Plan recommendations provided in this report:

- **Character Area Boundaries.** The Future Development Map’s character areas are conceptual and represent “approximate” location. Development proposals should not be constrained by rigid adherence to these conceptual boundaries.

- **Development Attributes.** The key development attributes included within each character area description serve as general guidance. They are not regulatory or static; nor, represent all measures of development form, scale, and intensity that must be considered when considering future development proposals. Rather, the highlighted attributes should be viewed as aspirational targets when reviewing development proposals or preparing land development regulations that may assist in the implementation of the Focus Area A Future Development Plan.

Key development attributes may also be subject to the following footnotes:

1. *Character Area Acreage.* Approximate as depicted on **Map II-B**. Actual acreage may vary.
2. *Building Height.* Measured in stories.
3. *Setback - Build-to-Lines.* Refers to street frontages only unless otherwise noted.
4. *Impervious Cover.* Including buildings, parking areas, and other impermeable portions of a parcel.
5. *Building Frontages.* Percentage of a parcel’s width that must be faced with a building facade at the front setback line.

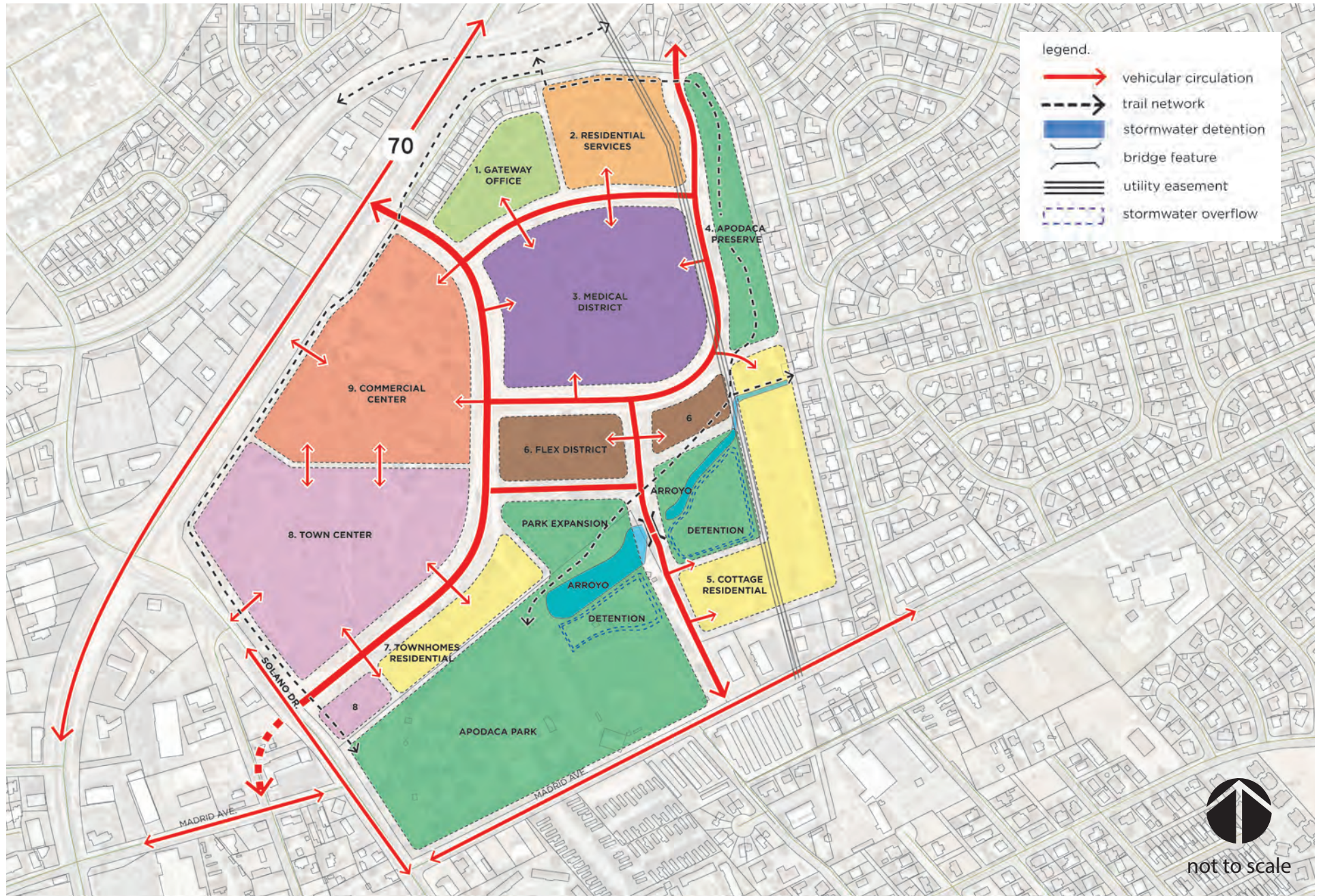
**FIGURE II.10, Focus Area A Preferences**



**FIGURE II.10:** Apodaca Blueprint survey results confirm residents’ desire for additional open space and recreational opportunities. Many respondents also view Focus Area A as a location that can generate jobs, and provide shopping and housing opportunities. Full survey results are provided in Appendix A.



# Map II-B, Focus Area A, Future Development Map





## 1. GATEWAY OFFICE



The Gateway Office character area serves as an inviting entry to Focus Area A, and supports development that provides a transition in scale between surrounding residential and non-residential land uses. The character area supports general office activities for a variety of health and human services, professional services, communications, finance, real estate, and related trades.

**Character.** Development scale will vary depending on arrangement – particularly regarding the proximity of development to the street and adjacent residences. Building footprints and heights may be greater where a consistent building façade is provided along the street edge, while smaller “garden” offices of residential scale may be permitted if development is set back from the street.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 5.1 Acres
- **BUILDING HEIGHT:** 2 stories max.; 1 story adjacent to residential.
- **SETBACK - BUILD-TO LINES:** May vary depending on building scale and height. Maximum setback of 10'-15' where building frontages are required.
- **BUILDING FRONTAGES:** Varies, but objective is to frame street and driveway edges.

## 2. RESIDENTIAL SERV.



The Residential Services character area provides a functional and aesthetic transition between adjacent neighborhoods and the Medical District character area. The character area is reserved for housing that provides residents with varying levels of custodial and health services including: independent living, assisted living, and skilled nursing facilities. This character area also supports services clearly intended to support the care of persons residing within the facility.

**Character.** Building type, scale, and form may vary depending on arrangement and proximity to surrounding land uses. Single story residential dwellings may be located along the northern and eastern edges of the character area while multi-story retirement homes and associated services should be located on the southern edge of the area. Flexibility on maximum height and intensity may be given to buildings that provide a consistent façade along the street edge.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 7.3 Acres
- **BUILDING HEIGHT:** 2 stories max.
- **SETBACK - BUILD-TO LINES:** May vary depending on building scale and height. Maximum setback of 10'-15' where building frontages are required.
- **BUILDING FRONTAGES:** Varies, but objective is to frame street and driveway edges.

## 3. MEDICAL DISTRICT



The Medical District character area provides for the development of a health care and medical campus, include hospitals and associated facilities for the evaluation, treatment, and care of patients. A hospital or other principal treatment facility may be supported by accessory, professional offices, ambulatory or outpatient care facilities, medical laboratories, and rehabilitative service facilities.

**Character.** Buildings adhering to the development targets described herein may be arranged in a suburban campus style of development with surface parking and substantial landscape buffers. Should additional development flexibility be requested, future square footage must be arranged to frame and address the street while surface parking must be removed in favor of structured parking and the development of urban civic spaces.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 20.6 Acres
- **BUILDING HEIGHT:** 2 stories max.
- **SETBACK - BUILD-TO LINES:** May vary depending on building scale and height. Maximum setback of 10'-15' where building frontages are required.
- **BUILDING FRONTAGES:** Varies, but objective is to frame street and driveway edges.

<sup>1</sup> Gross acreage. Approximate.

## 4. APODACA PRESERVE



The Apodaca Preserve is a linear open space corridor set aside for public use.

The character area buffers adjacent single-family residential development and links Apodaca Park with the Outfall Channel Trail. Some single-family detached or attached residential development may be permitted if directly adjacent to the Cottage Residential character area, and cumulatively encompassing no more than 10 percent of the Apodaca Preserve.

**Character.** Portions of this character area reserved for general open space may be largely left to natural reclamation, although restorative activities may take place to remove non-indigenous plants. Exceptions may include manicured areas adjacent to streets and multi-use trails; and, small “pocket park” locations designed for sitting, gathering, and limited activities as determined by the City of Las Cruces. Limited residential uses will be small-scale and clustered to minimize encroachment into the character area.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 4.6 Acres
- **BUILDING HEIGHT:** N/A. Structure height to be of a scale that is complimentary to adjacent residences.
- **SETBACK - BUILD-TO LINES:** Varies
- **BUILDING FRONTAGES:** N/A

## 5. COTTAGE RESID.



The Cottage Residential character area provides for small-lot single-family land uses and building types including patio homes and zero-lot-line construction. Some optional townhouse development may occur on the western edge of the character area, but should not exceed 50 percent of the total dwelling units. The character area serves as an extension of adjacent single-family neighborhoods into Focus Area A, but increases local housing choice by accommodating a more compact housing product than currently exists in residential areas to the east.

**Character.** Residential lots in the character area may be as little as 5,000 square feet to promote clustering and the reservation of open space that links Apodaca Park with the Apodaca Preserve character area. Front building facades should include porches, patios, or courtyards, while vehicular access and garages should be concealed from the street.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 13.1 Acres
- **BUILDING HEIGHT:** 1 story; except, that 2 stories may be permitted adjacent to Apodaca Park.
- **SETBACK - BUILD-TO LINES<sup>2</sup>:**  
Patio/Zero-Lot Line Homes: 10' - 15' front yard  
Townhomes: 5' - 15' front yard
- **IMPERVIOUS COVER:**  
Patio/Zero-Lot Line Homes: Up to 75 % of lot area.  
Townhomes: Up to 95% of lot area.

## 6. FLEX DISTRICT



The Flex District character area accommodates potential expansions of professional office, small-lot single family, and/or townhouse residential development depending on market conditions. The character area is divided into two sub-areas where differing intensities of development may be permitted.

**Character.** Flex District development characteristics will mimic the applicable Gateway Office, Cottage Residential, and Townhouse Residential attributes that are applied to each character area. Regardless of preferred land use, character area development adjacent to the Town Center and Commercial Center character areas must incorporate building and site design features complimenting the adjacent urban streetscape and facing buildings on the opposite side of the street.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 3.4 Acres
- **BUILDING HEIGHT:** 2 stories max.
- **SETBACK - BUILD-TO LINES:**  
Non-Residential: May vary depending on building scale and height. Maximum setback of 10'-15' where building frontages are required.  
Patio/Zero-Lot Line Homes: 10' - 15' front yard  
Townhomes: 5' - 15' front yard
- **IMPERVIOUS COVER:**  
Non-Residential: Varies  
Patio/Zero-Lot Line Homes: Up to 75 % of lot area.  
Townhomes: Up to 95% of lot area.

1. Gross acreage. Approximate. 2. Excludes building encroachments such as porches, stoops, stairs, etc.



## 7. TOWNHOUSE RESID.



The Townhouse Residential character area supports the development of attached housing units designed and arranged to form high-density blocks of urban residential character. Character area development compliments streets designed and constructed with an urban form to ensure a roadside environment that is engaging to residents and visitors.

**Character.** Attached housing units should be oriented to face the street and (where applicable) public park land. Building placement shall form a strong façade along the street and other public gathering spaces. Buildings may be two-to-three stories in height with taller units located to the west of the character area. Ground floor space for three-story units may serve as alley-accessed garage space, or may be converted to serve as an accessory unit or live-work space with street facing access.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 4.2 Acres
- **BUILDING HEIGHT:** 3 stories max.
- **SETBACK - BUILD-TO LINES<sup>2</sup>:** 5' - 15' front yard
- **IMPERVIOUS COVER:** Up to 95% of lot area.
- **BUILDING FRONTAGES:** Continuous street frontage

## 8. TOWN CENTER



The Town Center character area is a vibrant mix of residential and commercial uses, and supporting civic spaces where residents can live, shop, and recreate. The district is divided into a regular grid of streets and pedestrian pathways, with flanking buildings and activity spaces pulled close to the street to provide an engaging human scale.

**Character.** Mixed-use residential and commercial buildings are the predominant building type in the Town Center character area. These buildings will be built adjacent to urban streets with roadside edges that provide a comfortable atmosphere for the pedestrian and minimize motor vehicle volumes and speeds. On-street parking may be used to diminish parking requirements for surrounding land uses, while building and site arrangement allows for easy walking and bicycling access through the Town Center and into adjacent character areas and destinations.

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 21.9 Acres
- **BUILDING HEIGHT:** 3 stories max.; except that additional height may be considered for buildings bordering N. Solano Drive and N. Main Street.
- **SETBACK - BUILD-TO LINES<sup>2</sup>:** 5' - 10' (streets and driveways)
- **IMPERVIOUS COVER:** Up to 95% of lot area.
- **BUILDING FRONTAGES:** 75% minimum (streets and driveways)

## 9. COMMERCIAL CENTER



The Commercial Center character area accommodates general commercial uses including anchor retail and supporting shops and offices. The character area provides a uniform development character along N. Main Street including a consistent palette of building materials, signage, and landscaping.

**Character.** Development in the character area should be arranged to conceal parking behind retail establishments, and to orient the long-edge of buildings along streets and driveways. Street and driveway facing building facades should activate the adjacent roadside by providing high levels of fenestration, and pedestrian access. Out-parcel buildings on N. Main Street should likewise be pushed to the edge of a front yard clear zone and activate an adjacent multi-use trail and significant planting strip. Over time, the character area could accommodate intensification - replacing surface parking with additional buildings (and thereby extending the human-scale environment promoted in the adjacent Town Center character area).

### KEY DEVELOPMENT ATTRIBUTES

- **CHARACTER AREA ACREAGE<sup>1</sup>:** 17.4 Acres
- **BUILDING HEIGHT:** 2 stories max.
- **SETBACK - BUILD-TO LINES<sup>2</sup>:** Varies, but objective is to frame street and driveway edges.
- **BUILDING FRONTAGES:** Varies, but objective is to frame street and driveway edges.

1. Gross acreage. Approximate. 2. Excludes building encroachments such as porches, stoops, stairs, etc.

## BUILD-OUT SCENARIO

As represented by the Focus Area A Future Development Map (page 33), character area descriptions, and key development attributes, build-out potential for Focus Area A could include between 485 and 600 housing units. The total square footage of non-residential development could range between 460,000 and 580,000 square feet. The final development program and configuration may vary between adoption of the Blueprint and the submittal of development proposals for the site.

Efficient building design and site configurations may reveal unforeseen site development opportunities. In contrast, further site analysis or market research may limit build-out potential within one or more development nodes. Although many factors will influence ultimate build-out potential within Focus Area A, the following analyses will be critical in determining the degree to which the focus area vision is built:

- **Environmental Analysis.** In particular, will affirm the amount of on-site storm water detention that will be required to offset increases to impervious surface areas.
- **Market Analysis.** Changing market conditions will influence the viability of different housing types and densities on the site, and the maximum threshold for retail and office development.
- **Traffic Impact Analysis.** Will determine the potential impact of new development on existing street systems, and recommend ways to mitigate changes to traffic patterns.

**Table II.4, Focus Area A Build-Out Scenario<sup>12</sup>**

Character Area	Residential (Dwelling Units)	Dwelling Units per Gross Acre (DU/Acre)	Non-Residential (Square Feet)	Parks/Open Space <sup>2</sup>
1. Gateway Office	-	-	25,000 - 30,000	-
2. Residential Services	-	-	30,000 - 35,000	-
3. Medical District	-	-	130,000 - 160,000	-
4. Apodaca Greenbelt	-	-	-	3.8 acres
5. Cottage Residential	40 - 60	6.6 - 10	-	.3 acres
6. Flex District <sup>4</sup>	75 - 90 <sup>4</sup>	11.4 - 13.6	65,000 - 75,000 <sup>4</sup>	
7. Townhouse Residential	40 - 50	15.4 - 19.2	-	
8. Town Center <sup>5</sup>	330 - 400 <sup>5</sup>	20.5 - 24.8	40,000 - 80,000 <sup>5</sup>	2.2 acres
9. Commercial Center	-	-	115,000 - 140,000	2.7 acres
#. Apodaca Park <sup>6</sup>	-	-		6.0 acres
<b>TOTAL</b>	485 - 600	-	405,000 - 520,000	15.0 acres

1. Hypothetical only. Meant to represent a development target. Subject to change.

2. Build-out scenario assumptions based on based on Map II-C, Focus Area A Conceptual Site Plan (page 39). Assumes roughly 20 percent of the site converted to street right-of-way.

3. Based on principal parks/open space illustrated on page 39 only. Additional civic spaces and common areas (social space) may be required within individual character areas.

4. Residential and non-residential uses are not calculated cumulatively. Build-out assumes that the character area is developed solely for residential **or** non-residential use.

5. Total residential dwelling units assumes no ground floor commercial. Non-residential square footage includes up to 40,000 square feet of ground floor commercial in mixed use buildings.

Accounts for Hotel footprint as 15,000 square feet of ground floor commercial.

6. Potential Apodaca Park expansion, including dry detention area depicted between character areas 5 and 6 on Map II-C, Focus Area A Future Development Map (page 33).

## FOCUS AREA A, CONCEPTUAL SITE PLAN

Application of the recommended Focus Area A future land use and development features will require the creative use of the City's existing land development regulations and/or the preparation of new provisions. Administrative methods for implementing the Focus Area A vision are highlighted in the Apodaca Blueprint Action Plan beginning on page 62.

To further aid the City in implementing the spirit and intent of the Focus Area A future development vision, a conceptual site plan has been prepared which illustrates a hypothetical development on the former Las Cruces Country Club.

**The Focus Area A Conceptual Site Plan (Map II-C, facing page) is not a design mandate for the focus area.** The map and corresponding feature descriptions represent one of many possible development outcomes which incorporate the recommended building and site features presented in the Focus Area A future development plan. In addition to the Focus Area A Future Development Map, decision makers should consult the Conceptual Site Plan for additional guidance when considering development proposals within the focus area.

### CONCEPTUAL PLAN FEATURES

Future development proposals for property within Focus Area A should incorporate the following building and site design features:

- A. **Multi-Use Path Network.** An interconnected network of public multi-use trails should connect all character areas within the site, adjacent residential areas, Apodaca Park, and the regional trail network represented by the Outfall Channel Trail.
- B. **Public Recreation Space.** Public park land and/or open space should link Apodaca Park and the Outfall Channel Trail. Park expansions near Character Areas #5 and #8 may include programmed recreational facilities and manicured activity spaces. Park land and open space extensions to the east and north of Apodaca Park may include trail corridors, natural areas, and limited "neighborhood scale" play facilities; and may blend into adjacent private common areas.
- C. **Social Spaces.** Character areas #8 (Town Center) and #9 (Commercial Center) should incorporate a series of highly visible and accessible public gathering spaces such as greens, plazas, promenades, squares, etc. that can be used for formal events and activities, or informal use. Social spaces should be strategically located in areas where they can boost pedestrian activity in conjunction with surrounding buildings and land uses. Similar gathering spaces should be incorporated into private common areas located throughout the focus area.
- D. **Storm Water Detention Areas.** The City of Las Cruces may partner with private development interests to provide some or all storm water detention for the focus area within a publicly-dedicated regional facility that serves the neighborhood drainage basin. The design of detention areas should provide for additional recreational flex space for facilities such as multi-use fields.
- E. **Urban Streetscapes.** Major thoroughfares will incorporate narrow travel lanes, bicycle facilities, landscaped medians; and, wide roadsides zones with comfortable pedestrian pathways, furnishing zones, and landscape features. Urban thoroughfares will also accommodate on-street parking. Crossing facilities will provide comfortable and convenient pedestrian mobility.
- F. **Commercial Driveways.** Driveways within character area #9 (Commercial Center) should be spaced so that they approximate the scale of urban blocks (300' to 600'); and, should be aligned to serve as extensions of public streets. The driveways should mimic urban street characteristics that include designated bicycle facilities and roadside treatments that incorporate landscaping and comfortable pedestrian ways.
- G. **Transit Access.** Designated transit facilities should be provided on one or more thoroughfares within the focus area. An off-street transit mall may be incorporated within site at a central location.



# Map II-C, Focus Area A, Conceptual Site Plan

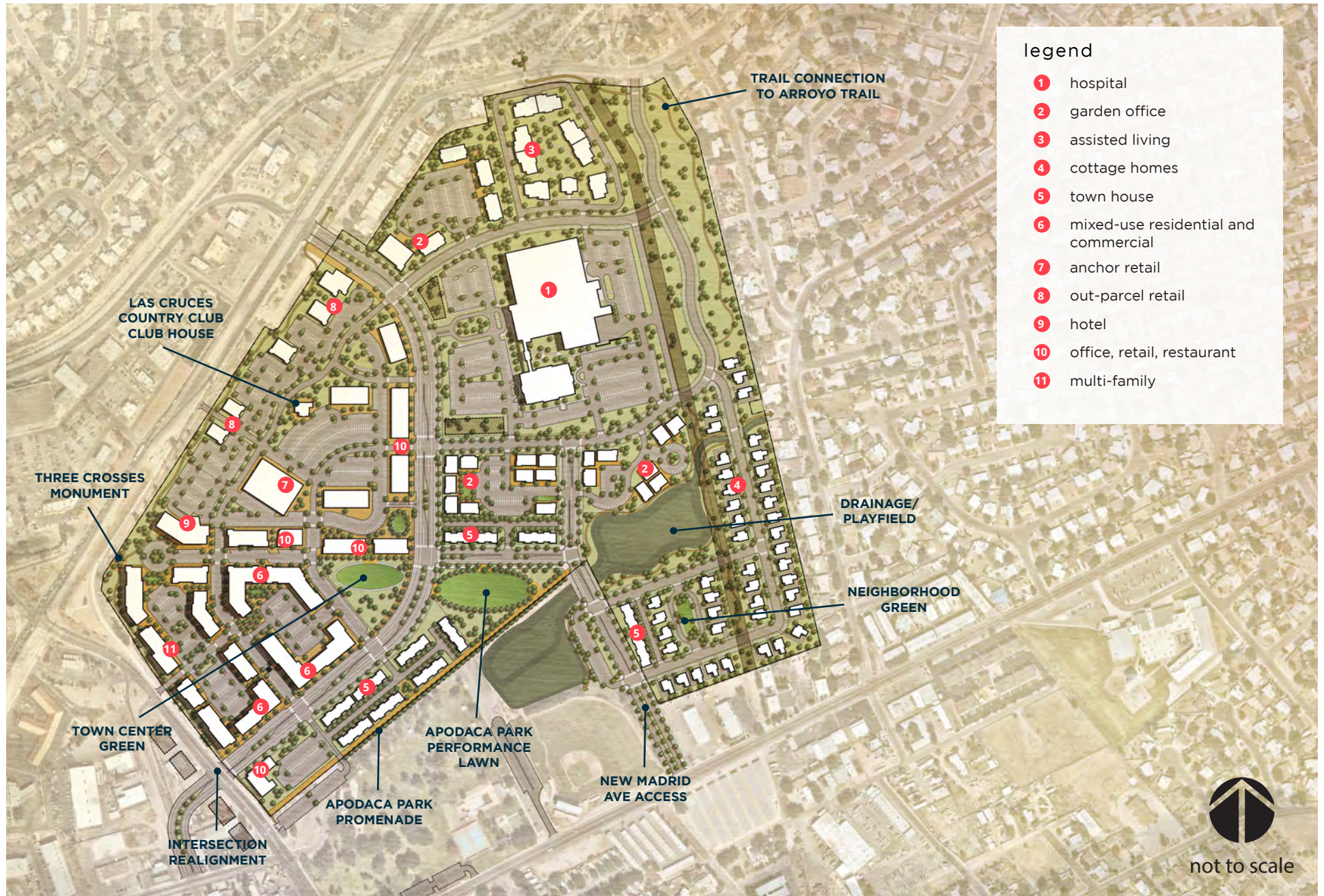






IMAGE 33: Storm Water detention facilities may be designed to provide extra multi-purpose field space. Laddie Place in San Antonio, Texas, currently allows for seven youth and/or two adult soccer fields.

- H. **Bicycle Facilities.** Bikeways of varying type should be incorporated into network streets, and into the cross-section of driveways that serve character area #9 (Commercial Center). Bicycle facilities should also take the form of intersection improvements that incorporate highly-visible traffic control devices including a mix of signage, pavement markings, traffic signal loops, and hand/actuated signals.
- I. **Off-street Parking.** Parking reductions should be permitted within character areas that support mixed-use development. As depicted in **Map II-C**, 1 residential parking space (on-street and off-street) is provided for each dwelling unit in character area #8 (Town Center). (The typical City multi-family parking requirement is 1.5 spaces per DU.)
- J. **Parking Disposition.** Off-street parking areas should not encroach into the frontage zone between the building and the street. Where possible, off-street parking should be located completely behind buildings; but, parking areas visible from the street should be screened by low walls and/or dense planting screens.
- K. **On-street Parking.** On-street parking should be incorporated into all public streets, and could be extended onto private access drives within one or more character areas. On-street parking should be included within the calculation when determining suitable area-wide parking ratios/requirements.
- L. **Frontage Zones (Town Center).** Buildings within Character Area #8 should frame the street edge. Seventy five percent or more of net frontage length along streets should consist of building facades. Build-to-lines may be selectively setback from the street where the space between the street and building façade is activated as a well-designed public civic space.
- M. **Frontage Zones (General Commercial).** Buildings within commercial retail centers and office parks should be placed to frame the street edge, and the edge of major internal access drives. At least 75 percent of the net frontage length along streets, and 40 percent of net frontage length along access drives, should consist of building facades.
- N. **Frontage Zones (Out-parcels).** Out-parcel buildings should be placed to frame the street edge. At least 40 percent of net frontage length along streets should consist of building facades; although clear zones in front of buildings may be of sufficient depth to accommodate significant planting strips and multi-use trails along major arterial roads.

Other associated Planning Area development topics are discussed beginning on page 56.



## REPRESENTATIVE IMAGES



*IMAGES 34 & 35: Social spaces, in the form of courtyards, greens, parkways, plazas, promenades, squares, or other private common areas, augment public parks in providing places where people can congregate and socialize. Such spaces should be included at high visibility locations within Focus Area development parcels.*

*IMAGE 36: Private drives bisecting commercial development parcels and providing access to parking areas should extend the public street system and mimic public street design.*





## REPRESENTATIVE IMAGES



IMAGE 39: The frontage zone, through way, and furnishing zone are essential roadside elements that make urban streets comfortable and promote activity.



IMAGES 40 & 41: Garden homes and townhouses are among the many housing types that may be incorporated into Focus Area A character areas.



IMAGES 37 & 38: Building frontages in urban and mixed use districts should form no less than 75 percent of the parcel width (37). Regardless of a district's minimum building frontage requirements, off-street parking should be framed by low walls or vegetation, and should not extend beyond the front building facade.





IMAGE 42: The Focus Area A future development plan does not make specific programming and space planning recommendations for Apodaca Park (other than the re-routing of traffic to the periphery of the property). Image 42 illustrates a possible re-arrangement of park space; but, Apodaca Park redevelopment should occur through a separate park master planning process..

IMAGE 43: The Focus Area A conceptual development plan suggests that the northern boundary of Apodaca Park be framed by a formal promenade and adjacent residential development. Although the scale of residential buildings pictured in Image 43 is greater than that envisioned for property adjacent to Apodaca Park, visual surveillance of the park can improve safety, while the park can increase the value of adjacent residences.



IMAGE 44: A multi-use trail segment developed in conjunction with a roadway enhancement and storm water detention facility.



**FOCUS AREA A: LAS CRUCES COUNTRY CLUB (EXISTING)**





FOCUS AREA A: LAS CRUCES COUNTRY CLUB PROPERTY (CONCEPT)





## Focus Area B: Villa Mora Property

The Villa Mora site is comprised of two (2) municipally-owned properties which include the remnants of one of the City’s original storm water levees. As Las Cruces has grown, the facility was replaced by a much larger earthen dam located on the opposite side of Interstate 25 from the Blueprint focus area. No longer necessary for storm water detention, the Villa

Mora levee has been bisected by a more recent extension of E. Madrid Avenue. Unlike Focus Area A, the Villa Mora property has not been the subject of substantial private development interest. Nonetheless, Apodaca Blueprint process has provided a public forum to explore community preferences regarding potential site development.

### SITE FEATURES

Figure II.11 (facing page) illustrates significant site features that have been observed during the Blueprint planning process. These features influence the future land use and development patterns recommended within Focus Area B.

Some key features include:

- **Berm.** The site’s defining feature buffers some of the residential neighborhoods to the west from noise and glare generated from traffic on Interstate 25 and development on the west side of the freeway. The berm also offers vista of surrounding mountain peaks – particularly those to the west of Las Cruces. The berm could also form part of an enhanced gateway into Apodaca-area neighborhoods.
- **Drainage/Ponding Areas.** The Villa Mora property’s sloping elevation – descending from east to west - results in intermittent ponding adjacent to the remaining berm.
- **Fairbanks Drive.** The extension of Fairbanks Drive to E. Madrid Avenue has created a lot “remnant” on the western edge of the focus area tracts. E. Madrid Avenue between Fairbanks Drive and Martha/Anita Drive is narrow but provides a visible transition between developed and undeveloped portions of the Apodaca planning area.

- **Interstate 25.** The site’s adjacency to Interstate 25 may limit the type of development that could occur on the site due to anticipated impacts from noise, and glare from development on the opposing side of the freeway.
- **Madrid/Triviz Intersection.** Traffic control investments at this intersection may facilitate improved access between the Triviz Trail and area neighborhoods. The potential long-term extension of E. Madrid Avenue under Interstate 25 to Telshor Avenue may provide the opportunity to construct grade-separated connections to and from the trail.
- **Triviz Trail.** Site enhancements could provide neighborhood residents with improved access to the Triviz Trail, enhancing its utility as a transportation corridor.
- **“Unimproved” Character.** The lack of pre-existing buildings, structures, and infrastructure on much of the site have resulted in its prolonged use as an informal park space. Portions of the site might be improved for recreational purposes.



IMAGE 45: As viewed from across Interstate 25, the Villa Mora property abuts the freeway and provides a physical buffer to planning area neighborhoods. IMAGE 46: Accessing the Villa Mora property from the west requires roadway users to traverse a narrow area at Martha and Anita Avenues.

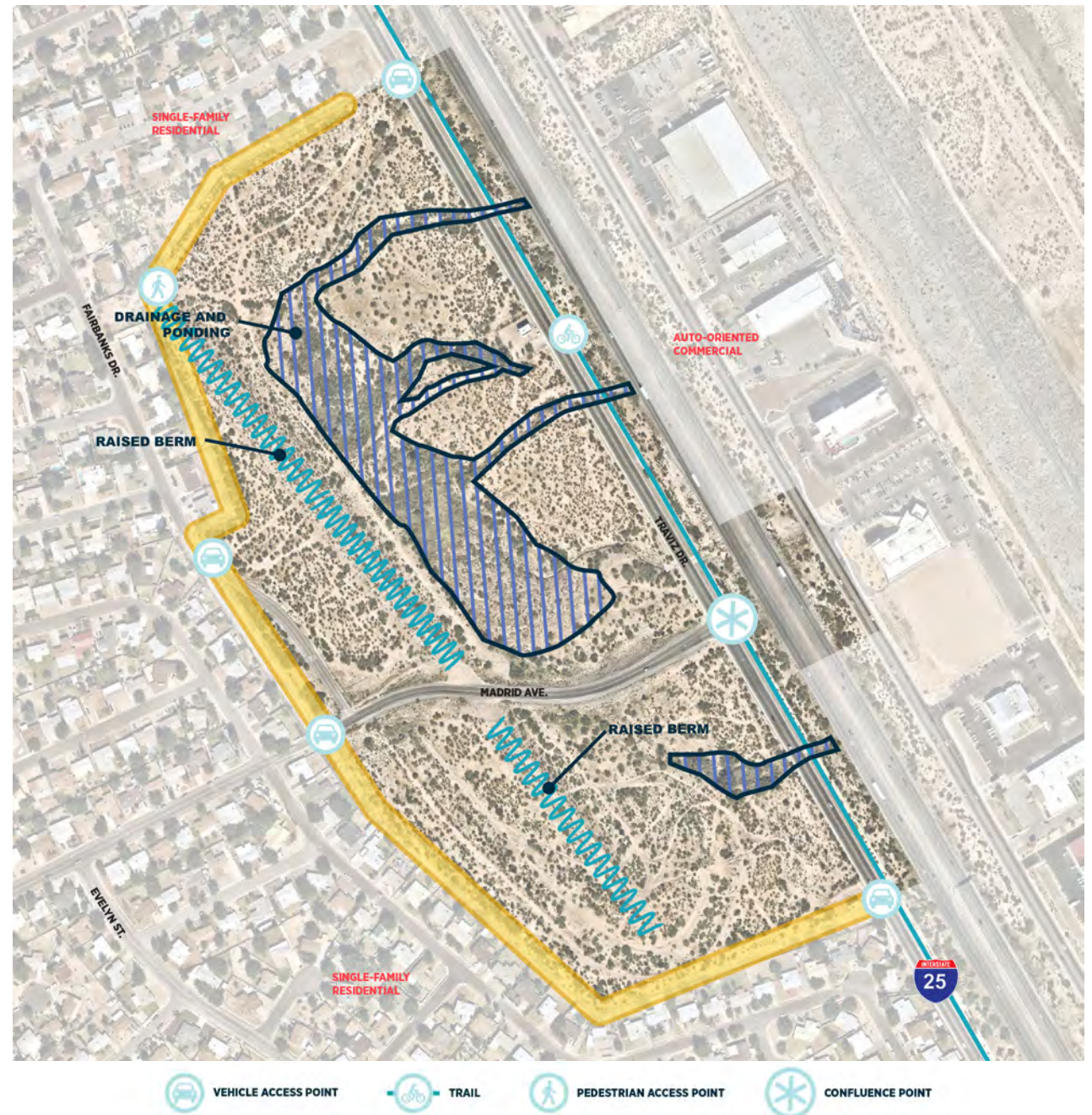




47

IMAGE 47: Remaining drainage infrastructure in front of the Villa Mora berm is no longer utilized, although low points on the site do experience periodic ponding.

Figure II.11, Focus Area B Site Features





## Focus Area B, Future Development Plan

The Apodaca Blueprint planning process generated a future development plan for Focus Area A that is more detailed than that proposed for other portions of the Blueprint planning area. The Focus Area B Future Development

Map (**Map II-D** facing page) depicts three “character areas” which establish basic land use and development characteristics for focus area properties. These character areas are described in more detail on page 50.

### INTERPRETING THE FUTURE DEVELOPMENT PLAN

As with the Apodaca Planning Area Future Development Map (page 15), the “character areas” depicted by the Focus Area B Future Development Map serve as a guide to City staff, and elected and appointed officials, when evaluating the merits of land use and development proposals for focus area property. When utilizing the Future Development Map and corresponding character area descriptions, municipal decision-makers are encouraged to exercise discretion on a case-by-case basis.

The following assumptions should be made when referencing the Focus Area B Future Development Plan recommendations provided in this report:

- Character Area Boundaries.** The Future Development Map’s character areas are conceptual and represent “approximate” location. Development proposals should not be constrained by rigid adherence to these conceptual boundaries.
- Character Descriptions.** The character descriptions included within each character area description are intended to provide general guidance. They are not regulatory or static; nor, represent all measures of development form, scale, and intensity that must be considered when considering future development proposals. Rather, the descriptions present aspirational targets when reviewing development proposals or preparing land development regulations that may assist in the implementation of the Focus Area B Future Development Plan.

**FIGURE II.12, Focus Area B Preferences**



*FIGURE II.12: Apodaca Blueprint survey respondents expressed a clear preference for more dedicated open space on the Villa Mora property. During other public outreach activities, some participants expressed that some limited site development may be suitable within the focus area.*



# Map II-D, Focus Area B, Future Development Map







## 1. RESTORATION PARK



The Restoration Park character area encompasses roughly 47 acres of Focus Area B. Intended to be largely dedicated for public use. Development within the character area would be dedicated to providing active and passive recreational opportunities for residents of surrounding neighborhoods, and public assembly or cultural/natural educational opportunities for a larger segment of the population.

**Character.** Much of the Restoration Park character area should be left un-manicured and reserved for native landscape restoration activities. Specific areas may be set aside for storm water enhancements, manicured and highly programmed active play spaces, and facilities that support public assembly. Specific facility programming for Restoration Park character area should be subject to a separate master planning process.

## 2. VILLA MORA PLAZA



The Villa Mora Plaza character area is a neighborhood commercial center that may support a mix of limited office and small retail establishments. The character area serves as an anchor to the western edge of the neighborhood and gateway to the Triviz Trail.

**Character.** Development scale within the Villa Mora character area is intended to remain limited - generally not exceeding a cumulative total of 25,000 to 30,000 gross square feet of enclosed space. Individual building footprints should not exceed 15,000 square feet and 1 story in height. Buildings should be pulled close to E. Madrid Avenue, but allow space for the Madrid and Triviz intersection to be dropped at a future date to extend under Interstate 25. Enclosed floor space should be augmented by outdoor plazas and patio space that alternatively serve as an enhanced trailhead or provide views to into the Restoration Park character area to the north and west.

## 3. DEVELOPMENT RES.



The Development Reserve character area is set aside for the potential long-term expansion of neighborhood commercial services or housing. The designation of this portion of Focus Area B as a “holding zone” reflects the potential for local conditions to change following development of the Villa Mora character area and the potential extension of E. Madrid Avenue to N. Telshor Boulevard.

**Character.** The construction of commercial or residential land uses in the Development Reserve character area should occur only after the two adjacent Focus Area B character areas have been fully developed according to the Blueprint development parameters. At that time, the potential use of the property should be re-evaluated to determine if it serve as an extension of the Restoration Park, or provide for commercial or residential uses. Potential commercial development should mimic the character of the Villa Mora Plaza character area while housing development may duplicate the applicable characteristics of the Cottage Residential character area that serves Focus Area A.



## REPRESENTATIVE IMAGES



IMAGES 49 & 50: Limited commercial development within Focus Area B may also serve as an important trailhead and public gathering spot for the Triviz Trail and larger regional trail network. Manicured landscape areas should be xeriscaped for drought tolerance and bio-filtration.

IMAGES 51 & 52: Public gathering facilities and open spaces within Focus Area B should relate to the restoration of native fauna within the City. "Restoration Park" could be dedicated to learning opportunities related to the area's natural and cultural heritage.



## FOCUS AREA B, CONCEPTUAL SITE PLAN

Application of the recommended Focus Area B future land use and development features will require the creative use of the City's existing land development regulations and/or the preparation of new provisions. Administrative methods for implementing the Focus Area B vision are highlighted in the Apodaca Blueprint Action Plan beginning on page 62 of this report.

To further aid the City in implementing the spirit and intent of the Focus Area B future development plan design vision, a conceptual site plan has been prepared which depicts a hypothetical illustration of completed development on the Villa Mora property.

**The Focus Area B Conceptual Site Plan (Exhibit II-E, facing page) is not a design mandate for the focus area.** The map and corresponding feature descriptions represent one of many possible development outcomes which incorporate the recommended building and site features presented in the Focus Area B future development plan. In addition to the Focus Area B Future Development Map, decision makers should consult the Conceptual Site Plan for additional guidance when considering property development proposals within the focus area.

### CONCEPTUAL PLAN FEATURES

Future development proposals for property within Focus Area B should incorporate the following building and site design features:

- A. **Active Transportation Connectivity.** Additions to the City's active transportation network such as sidewalks, multi-use trails, bicycle facilities, and intersection enhancements should run through the Focus Area - tying the neighborhood to the Triviz Trail. Distinctions should be made between active transportation facilities that serve as dual transportation/recreation function versus those recreational pathways that provide access to internal features within the Restoration Park character area.
- B. **Neighborhood Gateway.** Gateway features should be added to E. Madrid Avenue that provide a transition between Focus Area B and the Planning Area's Neighborhood Residential character area. Gateway features may include signage, medians, decorative lighting, and even a possible feature that spans existing berm.
- C. **Social Spaces.** Character area #2 (Villa Mora Plaza) should incorporate highly visible and accessible public gathering spaces such as greens, plazas, promenades, patios, etc. to support informal activities. Social spaces should be strategically located in areas where they can boost pedestrian activity in conjunction with surrounding buildings and land uses. Due to the proximity of the Restoration Park character area, these social spaces should predominantly remain in private ownership - integrated into the overall development site.
- D. **Frontage Zones.** Buildings within the Villa Mora Plaza character area should be placed to frame E. Madrid Avenue and N. Triviz Drive. At least 75 percent of the net frontage length along Madrid should consist of building façades. The ideal frontage requirement on N. Triviz Drive may vary (perhaps up to 40 percent). The build-to-line on both roadways should support wide clear zones for pedestrian facilities. Social spaces may also encroach with established clear zones.
- E. **Building Footprints.** Individual commercial building footprints should not exceed 15,000 square feet to promote the intended neighborhood scale of development.
- F. **Low Impact Development.** Buildings, streets, and site features should incorporate low impact development principles and elements including: green street infrastructure, permeable pavement, storm water trees and plantings, etc. The degree to which low impact development elements are applied to the Focus Area should consider the impacts to development costs and may vary at the discretion of the City.
- G. **Madrid-Triviz Intersection.** Although a high percentage of building frontage is recommended along E. Madrid Avenue, the clear zone between buildings and the street should be of sufficient depth to accommodate future improvements to the Madrid-Triviz intersection. Should E. Madrid Avenue be extended to N. Telshor Boulevard, the intersection could be lowered and potentially support grade-separated bicycle and pedestrian access between the Villa Mora Plaza character area and the Triviz Trail.



# Map II-E, Focus Area B, Conceptual Site Plan





**FOCUS AREA B: VILLA MORA PROPERTY (EXISTING)**





FOCUS AREA B: VILLA MORA PROPERTY (RESTORATION PLAZA CONCEPT)





# Apodaca Blueprint Action Plan

Pro-active implementation of the Apodaca Blueprint vision and objectives will extend beyond the planning area policies established in conjunction with the Future Development Map. Neighborhood conservation, reinvestment, and revitalization will require a mix operational changes, policies, regulations, studies, and financial investments. These implementation tools are described in **Figure II.13**.

## Implementation Categories



**FINANCIAL INVESTMENT.** Capital improvement or general fund expenditures to provide essential services or otherwise improve community quality of life. May include standard public revenue sources, unique public investment tools, and/or public-private partnerships.



**OPERATIONAL CHANGE.** Creation of new programs or modification of existing programs to address a public need. Possible adjustment of municipal staff roles, or changes to operational procedures to streamline the delivery of public services.



**POLICY.** Procedures used to guide City decisions, including policy statements or review criteria adopted by ordinance, by-laws for appointed boards, board-adopted policy statements, and administrative policy guides or standards manuals for municipal programs and activities.



**REGULATION.** Ordinances adopted by City Council to guide land development, and construction activities within Las Cruces. May also include associated design guidelines referenced within municipal code.



**STUDY.** Detailed study or investigation needed to determine the feasibility of a proposed development activity, municipal program, or public policy. Studies may be mandated for program development, or required to determine the appropriateness of

The Apodaca Blueprint Action Plan includes 24 recommended implementation actions that may be initiated and accomplished at any point following the adoption of the Blueprint. The Action Plan should, however, be viewed as a short-term work program with efforts made to initiate each action within the next five (5) years. Recommendations regarding Blueprint administration and updates are located on page 65.

Recommended Blueprint actions are grouped by topic on pages 56 through 61. All actions are summarized within **Table II.5** (page 62).

## TOPIC A: DEVELOPMENT STANDARDS

The Apodaca Blueprint provides City Council, appointed Boards and Commissions, and City staff with general direction regarding the preferred character of future development in the Apodaca planning area. Although this information may influence local decisions when considering future development applications, more definitive regulatory tools and design guidelines will be necessary to ensure consistent application of the Blueprint future development vision.

- **Action A.1: Planned Developments.** Utilize the City's "Special" Zoning Districts to ensure that Focus Area A and B development is consistent with the Apodaca Blueprint future development vision. Absent new transect zoning standards (Action A.2) use the City's Infill Overlay District or Planned Unit Development District to consider focus area development applications.

- **Action A.2: Transect Zoning Standards.** Prepare a series of form-based zoning districts which mimic elements of the Downtown Development Code, and promote mixed-uses and pedestrian-friendly building and site design at varying scales. Promote the use of these "parallel" zoning standards within applicable portions of the Apodaca Blueprint planning area and other areas of the City on a case-by-case basis.
- **Action A.3: Corridor Design Districts.** Prepare corridor design overlay districts that implement the character area and character corridor design principles and policies promoted in the Blueprint. The overlays should be prepared under the assumption that they may be applied to multiple corridors over time. Adoption of uniform corridor overlays can reduce confusion that can occur through the presence of multiple districts with unique standards.

## AUTO-ORIENTED LAND USES

To help generate a walkable neighborhood character, and promote the Planning Area design vision, auto-oriented land uses such as drive-thru facilities, gas stations, and motor vehicle service establishments should largely be avoided. Exceptions may be considered for the Gateway character area and portions of Solano Drive, but even in these locations auto-oriented facilities should be screened from the street via the strategic placement of buildings and parking, and screened from surrounding residential areas.



## DRIVEWAYS AND ALLEYS

The comfortable pedestrian environment that is promoted within the Blueprint document through the development of urban street standards can easily be disrupted by repetitive driveway approaches to adjacent parking areas. In addition, narrower street widths and curb radii can often cause push back from local emergency service providers. Both issues can be mitigated through the selective consolidation of driveways, and corresponding provision of cross-access drives and public alleys at the rear of property. Consolidated driveways reduce the number of motor vehicle/pedestrian conflict zones and increase roadway level of service, while rear alleys of cross-access drives provide a secondary set of parallel “thoroughfares” that provide efficient access to the rear of property.



- **Action A.4: Alternative Street Standards.** Amend the Las Cruces Development Code to create an urban street area within the City, and adopt corresponding alternative street standards (including alleys). Include language regarding the application of alternative street standards to existing corridors over time.

## ACTIVE TRANSPORTATION

Some of the exhibits in the Apodaca Blueprint illustrate bicycle, pedestrian, and shared use facilities and recommend network expansion. (The specific facilities exhibited in the Blueprint are suggestive only. Other suitable facility options may be considered by the City.)

Active transportation facilities adhering to applicable guiding City documents should be incorporated into the Planning Area’s transportation network. Network expansion may occur as part of new development or a new capital project. The specific active transportation facilities constructed throughout the Planning Area should promote the Blueprint’s character area and character corridor design recommendations.



- **Action A.5: Active Transportation Facilities.** Amend the Las Cruces Development Code to include expanded provisions on active transportation facility design including an expanded inventory of bikeway, pedestrian pathway, and shared-use facilities. Active transportation facility standards should also address intersections and other crossings, as well as traffic control devices. Active transportation standards should differentiate between areas designated for standard street and urban street development.

## TOPIC B: STREET SYSTEM

The Apodaca Blueprint presents a modified approach to street network arrangement and street design than is currently promoted through the City’s land development regulations. Focus Area conceptual development plans also suggest that street reconfiguration may be appropriate in some locations within the Planning Area. A series of capital projects may be required to proactively advance the Blueprint’s street system recommendations.

- **Action B.1: Madrid/Mesquite/Solano Intersections.** Commission a feasibility study to evaluate street closure and realignment options at N. Mesquite Street, E. Madrid Avenue, and N. Solano Drive that may enhance access to Focus Area A and improve traffic flow for vehicles accessing US 70. Feasibility study funding may be subject to the public-private partnership opportunities identified in Action Plan Topic F, or may be required by the City as a condition of approval for a formal Focus Area A development submittal.
- **Action B.2: E. Madrid Avenue.** Prepare a schematic design for E. Madrid Avenue between N. Solano Drive and Martha Drive that applies the Blueprint’s street system and character corridor design recommendations, and including access management, traffic calming, streetscaping, storm water drainage, transit facilities, and active transportation facilities. Program phased implementation.



- **Action B.3: N. Solano Drive.** Prepare a feasibility study and schematic design for N. Solano Drive between US 70 and Spruce Avenue that applies the Blueprint’s street system and character corridor design recommendations, and including access management, traffic calming, streetscaping, storm water drainage, transit facilities, active transportation facilities, and possible lane reductions. Program phased implementation.
- **Action B.4: Street Reconfigurations.** In conjunction with E. Madrid Avenue and N. Solano Drive, and potential focus area development, prepare schematic designs of selected street realignments and/or closures that support the Blueprint’s street network recommendations. Identify preferred alternatives and prioritize one or more associated projects for inclusion in the City’s capital improvements plan.
- **Action B.5: Madrid/Triviz Intersection.** Determine the types of improvements that must be made to establish a controlled intersection at E. Madrid Avenue and Triviz Drive. Consider traffic control devices, turn lanes, bicycle and pedestrian crossing enhancements, and sidewalk improvements extending west along E. Madrid Avenue. Schedule applicable (and short-term) capital improvements, and consider long-term conceptual designs associated with a possible connection to Telshor Boulevard that create a grade separated crossing for bicyclists and pedestrians accessing the Triviz Trail.

## PUBLIC TRANSIT

RoadRUNNER public transit operates bus routes that bisect and include stops within the Apodaca Planning Area. As proposed within the Blueprint, the development intensity proposed within Focus Area A (and corresponding job potential) can provide a valuable anchor to RoadRUNNER transit routes that may link the Planning Area to downtown. To take advantage of this opportunity, Focus Area A development should include at least one “enhanced” transit stop within the core of the property. An enhanced transit stop refers to a stop that is well-designed, comfortable, convenient, and inviting for potential users. Within Focus Area A, the enhanced transit stop may take the form of:

- A bus-only turn-out located within Development District Commercial Center or Town Center development district that may be accessed by a public street; or,
- A pair of bus turn-outs (serving traffic in both directions) located on a Boulevard and at a central location within the Focus Area.

In addition to the principal enhanced transit stop in this section, the City should consider other locations within the Planning Area where transit facilities may be added or upgraded.



- **Action B.6: Transit Stops.** Design and schedule the installation of enhanced transit stops on N. Solano Drive and E. Madrid Avenue including shelters and bus turn-outs.

## TOPIC C: PARKS AND OPEN SPACE

Apodaca Blueprint participants expressed a clear desire to incorporate additional publicly-dedicated open space as part of future neighborhood development and revitalization activities. Preferences do not relate solely to the amount of public park land and open space that is available to Planning Area residents, but also the accessibility of existing recreational facilities.

- **Action C.1: Apodaca Park Master Plan.** Prepare a master plan for Apodaca Park that provides for park rehabilitation, and considers updates to park programming and facilities through a public input process. Include possible park expansion areas as identified in the Blueprint (including potential regional detention flex space). Prepare a comprehensive schedule of proposed improvements including cost estimates and phasing.
- **Action C.2: Villa Mora Park Master Plan.** Prepare a master plan for a new public park in portions of Focus Area B as identified in the Blueprint. Identify park programming and facilities through a public input process. Prepare a comprehensive schedule of proposed improvements including cost estimates and phasing.

## SOCIAL SPACE

Contemporary development has long favored the provision of inward looking private residential spaces located behind a series of garage doors and privacy fences; and, air-conditioned work and shopping destinations that filter us from our surrounding environments. Our insular pattern of development has come at the cost of our shared public realm.

New development within the Apodaca Planning Area should be accompanied by a variety of formal common areas for public gathering (such as courtyards, greens, parkways, plazas, promenades, or squares) and/or private recreational spaces – both of which will augment the publicly dedicated parks and open spaces. Private social spaces must be located in high visibility and accessible areas – often forming extensions of the public realm. Private recreational facilities should provide amenities that are approved by the City and meet identified recreational deficiencies in the community.

- **Action C.3: Trail Network Connections.** Prepare and prioritize a list of trail access projects that will result in additional/enhanced trail network connections between established neighborhoods and the Outfall Channel Trail, Triviz Trail and Blueprint focus areas.



## TOPIC D: STORM WATER DETENTION

The loss of natural ground cover associated with new development can exacerbate localized flooding issues that can occur during cyclical rain events. Certain storm water collection techniques - from regional detention facilities to green infrastructure - can mitigate these concerns. The selective use of both techniques can provide communities with greater flexibility in the intensity and form of development that they may wish to promote for long-term sustainability.

- **Action D.1: Pilot Project.** Incorporate “green street” principles into streetscape or street rehabilitation projects where possible to demonstrate concept feasibility. Madrid Avenue may serve as a suitable pilot location in conjunction with possible active transportation enhancement, resurfacing, and/or street-scaping.
- **Action D.2: Low Impact Development Standards.** Draft low impact development (LID) provisions for inclusion into applicable chapters of the Las Cruces Development Code. Provisions should incentivize the use of LID design features on private property and new streets.
- **Action D.3: Regional Detention.** Consider the construction of regional detention facilities within Apodaca Park to serve the neighborhood-wide drainage basin. Regional detention may be located within potential park expansion zones.

## STORM WATER DETENTION

Rain events in the arid climate of southern New Mexico are infrequent throughout any given calendar year. Although the combination of seasonal rain storms and sparse vegetation can create sudden and severe flash flooding, Las Cruces’ storm water detention facilities are rarely inundated with standing water.

The City should consider the development of regional storm water detention facilities within the Planning Area that can also be used as flex space for the provision of multi-use fields. Design features and regular maintenance can ensure residents that public recreation is the primary purpose of the flex facility, while storm water detention is merely an intermittent function.



## LOW IMPACT DEVELOPMENT

Low impact development combines on-site features, and green streets infrastructure including: bio-retention and bio-filtration planters, bio-retention swales, and hybrid bio-retention planters. The use of permeable paving materials within parking lots and driveway aisles can expand the cumulative enhancements to collecting and treating storm water runoff. With selected tree species, other vegetation, and ground covers, low-impact development infrastructure and technology can be optimized for regional climates and ecology.





## TOPIC E: NEIGHBORHOOD CONSERVATION

As the Apodaca Planning Area's built environment has aged and changed, some properties have exhibited advanced signs of disinvestment and deferred maintenance. Likewise, the timely maintenance and upkeep of public infrastructure becomes more challenging as the community's infrastructure systems and utility networks have expanded.

- **Action E.1: Code Enforcement Action Team.** Establish a code enforcement action team comprised of neighborhood association representatives that meet with City code enforcement officials on a recurring basis to review the status of neighborhood-wide code compliance requests. The Action Team program should be limited to neighborhood associations that have completed a City registration process and maintained sufficient records.
- **Action E.2: Pavement Preservation Program.** Develop a new pavement preservation program for Planning Area streets and schedule pro-active maintenance on street groupings where the condition does not necessitate a capital project.
- **Action E.3: Traffic Calming Evaluation.** Study the effects of traffic calming features that have been added to Planning Area neighborhoods to assess speeds, volumes, and changes to routes. Determine if alternative or additional features should be applied to local streets.

### UNIFORM DESIGN FEATURES

The long-term value of investment within potential development tracts such as Focus Area A and Focus Area B is influenced by the care taken in initial building and site design. In particular, a uniform set of building and site design criteria must be established and approved by the City prior to the approval of any conceptual site plans submitted under the provisions of the Las Cruces Development Code.

Architectural details and elevations must identify appropriate building materials, façade features, fenestration, orientation, screening, signage, etc. Consistent standards must be applied to other sight structures such as signs, retaining walls, pavement treatments, and more. Specific architectural styles may be established as well. Above all, corporate branding and individual preferences should not be allowed to circumvent the agreed-upon palette for development zone to avoid a haphazard approach to property development.

- **Action E.4: Special Financing Districts.** Evaluate the feasibility of jump-starting planned Planning Area infrastructure improvements through the establishment of a Special Assessment District or other similar state-authorized funding tool. Poll neighborhood property owners on possible participation.



### AFFORDABLE HOUSING

The Apodaca Planning Area contains 5 of the City's 19 affordable housing complexes - developments that include maximum household income requirements for residency. High concentrations of affordable housing within constrained geographic areas can produce cumulative negative impacts on the stability of surrounding neighborhoods. A policy of affordable housing dispersal should be practiced instead, with a ratio of no greater than 1 subsidized or affordable housing unit per 10 total dwelling units. This pattern of dispersal should be applied separately to low-density residential areas and multi-family housing complexes.

- **Action E.5: Neighborhood Branding.** Through a public process, create a brand for the neighborhoods of the Apodaca Planning Area. Incorporate the brand on public signage, as a possible element of private signage, and as part of an accompanying way-finding program.



## TOPIC F: INFRASTRUCTURE FINANCING

Effective implementation of the future development vision presented in the Apodaca Blueprint will require the pro-active participation of the City of Las Cruces to ensure that the scale, design, and timing of public infrastructure supports anticipated private investments. In particular, the need for City participation in planning area development/redevelopment activities is anticipated in relation to Focus Area A development. Creative financing may be required to fund public infrastructure development in the planning area.

- **Action F.1: Tax Increment Development District.** Identify and fund infrastructure that capitalizes on, or mitigates the impacts of, substantial investment projects in the Apodaca planning area. This action is contingent on favorable findings regarding anticipated returns on investment activity.
- **Action F.2: Development Agreements.** As an alternative to a TIDD, consider the execution of development agreements with private partners to fund infrastructure related to substantial investment projects in the Apodaca planning area. Adopt a policy to establish development agreement parameters. (See the City of Rio Rancho's "Gross Receipts Investment Policy," Rio Rancho Code of Ordinances, §§ 36.75 - 36.82)

### APODACA PLANNING AREA TIDD

The boundaries of an Apodaca TIDD should extend beyond the former Las Cruces Country Club property to fund off-site improvements which may support on-site development or mitigate anticipated development impacts. Extension of the TIDD boundaries (below) to include surrounding thoroughfares may also support reinvestment along these important planning area corridors.



#### possible improvements














- 1 Storm water detention
- 2 Trail access
- 3 Street reconfigurations
- 4 New street



*The TIDD boundaries presented in this map are hypothetical. No TIDD has been proposed in the Apodaca planning area as of the date of Blueprint adoption.*

























**Table II.5, Apodaca Blueprint Action Plan Summary**

Action By Topic		Implementation Category <sup>1</sup>				
<b>Topic A. Development Standards</b>						
<b>A.1</b>	<b>Planned Developments.</b> Utilize the City’s “Special” Zoning Districts to ensure that Focus Area A and B development is consistent with the Apodaca Blueprint future development vision. Absent new transect zoning standards (Action A.2) use the City’s Infill Overlay District or Planned Unit Development District to consider focus area development applications.					
<b>A.2</b>	<b>Transect Zoning Standards.</b> Prepare a series of form-based zoning districts which mimic elements of the Downtown Development Code, and promote mixed-uses and pedestrian-friendly building and site design at varying scales. Promote the use of these “parallel” zoning standards within applicable portions of the Apodaca Blueprint planning area and other areas of the City on a case-by-case basis.					
<b>A.3</b>	<b>Corridor Design Districts.</b> Prepare corridor design overlay districts that implement the character area and character corridor design principles and policies promoted in the Blueprint. The overlays should be prepared under the assumption that they may be applied to multiple corridors over time. Adoption of uniform corridor overlays can reduce confusion that can occur through the presence of multiple districts with unique standards.					
<b>A.4</b>	<b>Alternative Street Standards.</b> Amend the Las Cruces Development Code to create an urban street area within the City, and adopt corresponding alternative street standards (including alleys). Include language regarding the application of alternative street standards to existing corridors over time.					
<b>A.5</b>	<b>Active Transportation Facilities.</b> Amend the La Cruces Development Code to include expanded provisions on active transportation facility design including an expanded inventory of bikeway, pedestrian pathway, and shared-use facilities. Active transportation facility standards should also address intersections and other crossings, as well as traffic control devices. Active transportation standards should differentiate between areas designated for standard street and urban street development.					
<b>Topic B. Street System</b>						
<b>B.1</b>	<b>Madrid/Mesquite/Solano Intersections.</b> Commission a feasibility study to evaluate street closure and realignment options at N. Mesquite Street, E. Madrid Avenue, and N. Solano Drive that may enhance access to Focus Area A and improve traffic flow for vehicles accessing US 70. Feasibility study funding may be subject to the public-private partnership opportunities identified in Action Plan Topic F, or may be required by the City as a condition of approval for a formal Focus Area A development submittal.					
<b>B.2</b>	<b>E. Madrid Avenue.</b> Prepare a schematic design for E. Madrid Avenue between N. Solano Drive and Martha Drive that applies the Blueprint’s street system and character corridor design recommendations, and including access management, traffic calming, streetscaping, storm water drainage, transit facilities, and active transportation facilities. Program phased implementation.					

1.  financial investment,  operational change,  policy,  regulation,  study. See Figure II.13 (page 56) for descriptions.


























**Table II.5, Apodaca Blueprint Action Plan Summary**

Action By Topic		Implementation Category <sup>1</sup>				
<b>B.3</b>	<b>N. Solano Drive.</b> Prepare a feasibility study and schematic design for N. Solano Drive between US 70 and Spruce Avenue that applies the Blueprint’s street system and character corridor design recommendations, and including access management, traffic calming, streetscaping, storm water drainage, transit facilities, active transportation facilities, and possible lane reductions. Program phased implementation.					
<b>B.4</b>	<b>Street Reconfigurations.</b> In conjunction with E. Madrid Avenue and N. Solano Drive, and potential focus area development, prepare schematic designs of selected street realignments and/or closures that support the Blueprint’s street network recommendations. Identify preferred alternatives and prioritize one or more associated projects for inclusion in the City’s capital improvements plan.					
<b>B.5</b>	<b>Madrid/Triviz Intersection.</b> Determine the types of improvements that must be made to establish a controlled intersection at E. Madrid Avenue and Triviz Drive. Consider traffic control devices, turn lanes, bicycle and pedestrian crossing enhancements, and sidewalk improvements extending west along E. Madrid Avenue. Schedule applicable (and short-term) capital improvements, and consider long-term conceptual designs associated with a possible connection to Telshor Boulevard that create a grade separated crossing for bicyclists and pedestrians accessing the Triviz Trail.					
<b>B.6</b>	<b>Transit Stops.</b> Design and schedule the installation of enhanced transit stops on N. Solano Drive and E. Madrid Avenue including shelters and bus turn-outs.					
<b>Topic C. Parks and Open Space</b>						
<b>C.1</b>	<b>Apodaca Park Master Plan.</b> Prepare a master plan for Apodaca Park that provides for park rehabilitation, and considers updates to park programming and facilities through a public input process. Include possible park expansion areas as identified in the Blueprint (including potential regional detention flex space). Prepare a comprehensive schedule of proposed improvements including cost estimates and phasing.					
<b>C.2</b>	<b>Villa Mora Park Master Plan.</b> Prepare a master plan for a new public park in portions of Focus Area B as identified in the Blueprint. Identify park programming and facilities through a public input process. Prepare a comprehensive schedule of proposed improvements including cost estimates and phasing.					
<b>C.3</b>	<b>Trail Network Connections.</b> Prepare and prioritize a list of trail access projects that will result in additional/enhanced trail network connections between established neighborhoods and the Outfall Channel Trail, Triviz Trail and Blueprint focus areas.					
<b>Topic D. Storm Water Detention</b>						
<b>D.1</b>	<b>Pilot Project.</b> Incorporate “green street” principles into streetscape or street rehabilitation projects where possible to demonstrate concept feasibility. Madrid Avenue may serve as a suitable pilot location in conjunction with possible active transportation enhancement, resurfacing, and/or street-scaping.					

1.  financial investment,  operational change,  policy,  regulation,  study. See Figure II.13 (page 56) for descriptions.



**Table II.5, Apodaca Blueprint Action Plan Summary**

Action By Topic		Implementation Category <sup>1</sup>				
<b>D.2</b>	<b>Low Impact Development Standards.</b> Draft low impact development (LID) provisions for inclusion into applicable chapters of the Las Cruces Development Code. Provisions should incentivize the use of LID design features on private property and new streets.					
<b>D.3</b>	<b>Regional Detention.</b> Consider the construction of regional detention facilities within Apodaca Park to serve the neighborhood-wide drainage basin. Regional detention may be located within potential park expansion zones.					
<b>Topic E. Neighborhood Conservation</b>						
<b>E.1</b>	<b>Code Enforcement Action Team.</b> Establish a code enforcement action team comprised of neighborhood association representatives that meet with City code enforcement officials on a recurring basis to review the status of neighborhood-wide code compliance requests. The Action Team program should be limited to neighborhood associations that have completed a City registration process and maintained sufficient records.					
<b>E.2</b>	<b>Pavement Preservation Program.</b> Develop a new pavement preservation program for Planning Area streets and schedule pro-active maintenance on street groupings where the condition does not necessitate a capital project.					
<b>E.3</b>	<b>Traffic Calming Evaluation.</b> Study the effects of traffic calming features that have been added to Planning Area neighborhoods to assess speeds, volumes, and changes to routes. Determine if alternative or additional features should be applied to local streets.					
<b>E.4</b>	<b>Special Financing Districts.</b> Evaluate the feasibility of jump-starting planned Planning Area infrastructure improvements through the establishment of a Special Assessment District or other similar state-authorized funding tool. Poll neighborhood property owners on possible participation.					
<b>E.5</b>	<b>Neighborhood Branding.</b> Through a public process, create a brand for the neighborhoods of the Apodaca Planning Area. Incorporate the brand on public signage, as a possible element of private signage, and as part of an accompanying way-finding program.					
<b>Topic F. Infrastructure Financing</b>						
<b>F.1</b>	<b>Tax Increment Development District.</b> Partner with private development entities to fund infrastructure related to substantial investment projects in the Apodaca planning area. This action is contingent on favorable findings regarding anticipated returns on investment activity.					
<b>F.2</b>	<b>Development Agreements.</b> As an alternative to a TIDD, consider the execution of development agreements with private partners to fund infrastructure related to substantial investment projects in the Apodaca planning area. Adopt a policy to establish development agreement parameters. (See the City of Rio Rancho’s “Gross Receipts Investment Policy,” Rio Rancho Code of Ordinances, §§ 36.75 - 36.82)					

1.  financial investment,  operational change,  policy,  regulation,  study. See Figure II.13 (page 56) for descriptions.

## MONITORING THE BLUEPRINT

Implementation of the Apodaca Blueprint must accommodate shifts in social, economic, physical, and political changes in Las Cruces over time. It is recommended that the Plan be revisited on a routine basis followed by warranted updates. Similar to its role in relation to comprehensive plan preparation and oversight (Las Cruces Municipal Code, § 2-381), Blueprint monitoring activities could be assumed by the Planning and Zoning Commission. Blueprint monitoring activities may include:

- **Progress Report.** The Planning and Zoning Commission should prepare a bi-annual report for submittal to City Council providing an update of Blueprint implementation. Significant actions and accomplishments during the preceding two-year period would be recognized, in addition to recommended amendments.
- **Blueprint Updates.** After the bi-annual (or annual) progress report is prepared and accepted by City Council, amendments to the Blueprint's Future Development Map and Action Plan may be considered. Identification of potential Blueprint amendments should be an ongoing process by the Planning and Zoning Commission and City staff throughout the Blueprint's implementation period. Proposed amendments should be reviewed and approved by the Planning and Zoning Commission, and adopted by City Council in a manner similar to the original Blueprint adoption.

IMAGES 53 & 54: Renderings of successful Blueprint implementation within Focus Area A (53) and Focus Area B (54).





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# APODACA BLUEPRINT



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